



Pilning & Severn Beach

Neighbourhood Area
Design Codes and Guidelines

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Quality information



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Introduction

01

1. Introduction

This section provides context and general information to introduce the project and its location.

AECOM has been commissioned to provide design support to the Pilning & Severn Beach Neighbourhood Plan Steering Group, through the Department for Levelling Up, Housing and Communities (DLUHC) - funded Neighbourhood Planning Programme, led by Locality. This document has been produced to inform new residential (only) development proposed in the Pilning & Severn Beach Neighbourhood Area. It presents a summary of the key characteristics which make this a special place to live and visit and this information is used to inform specific Design Codes and Guidelines which promote sustainable development and guide best practice.

The approach set out here is supported by the National Planning Policy Framework (NPPF), which encourages local authorities to consider using design codes, to help deliver high quality outcomes for new development.

It is important however, that guidance finds the balance between promoting and reinforcing local distinctiveness and allowing for innovation and originality. The NPPF suggests that 'design policies should be developed with local communities, so they reflect local aspirations and are grounded in an understanding and evaluation of each area's defining characteristics' (NPPF, 2021).

The NPPF also emphasises that 'the creation of high-quality, beautiful and sustainable buildings and places is fundamental to what the planning and development process should achieve. Good design is a key aspect of sustainable development, creates better places in which to live and work and helps make development acceptable to communities' (NPPF, 2021). It is therefore important that planning policies and decisions address the connection between people and places and how any new residential development will respond to and integrate successfully into the natural, built and historic environment.

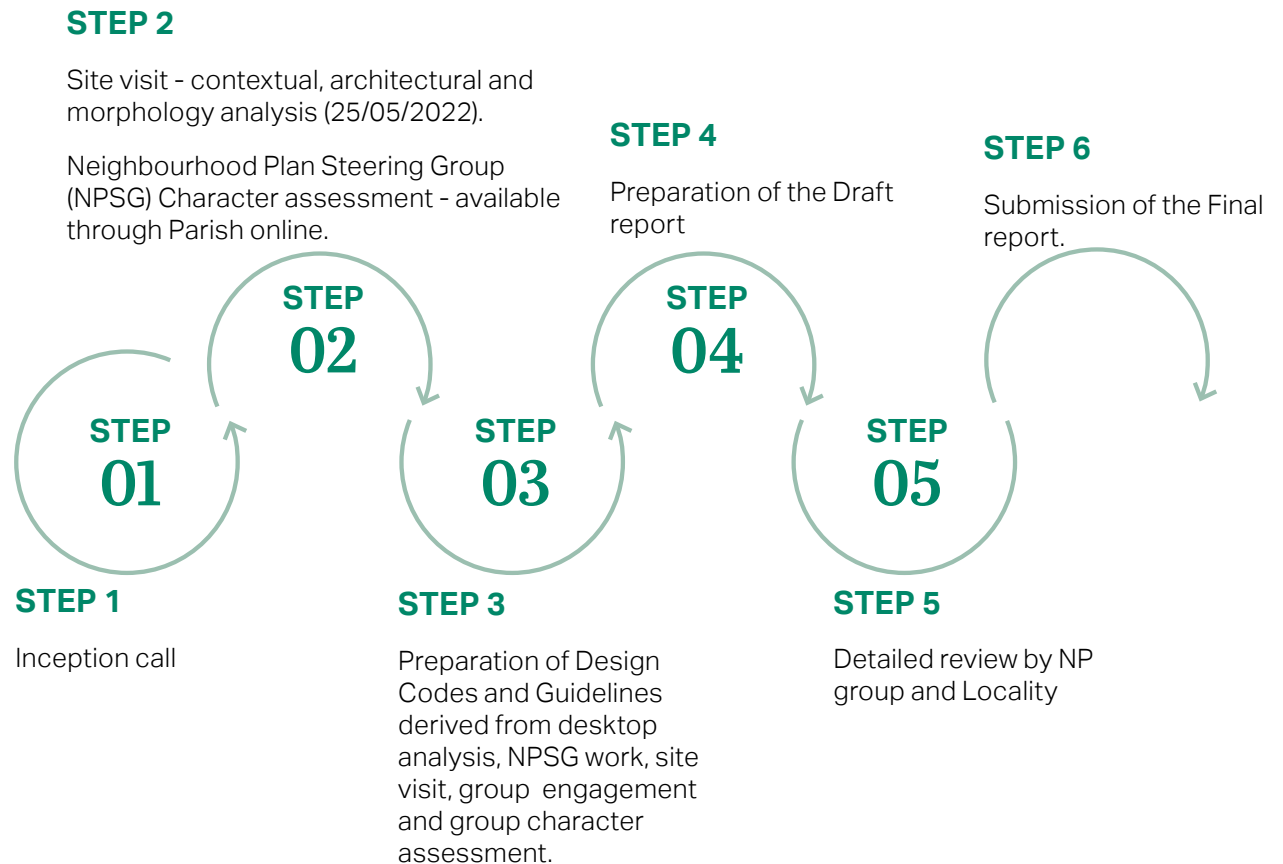
1.1 Objectives

The main objective of this document is to establish principles to ensure new residential development is designed and planned according to existing character and the context of Pilning & Severn Beach. It sets out a series of Design Codes and Guidelines related to residential development.

The document initially provides context 'character assessment' to the Design Codes and Guidelines including strategic issues identified by Neighbourhood Plan Steering Group together with the aspirations of the community, as although not strictly design issues, these must be considered in the context of any design proposal.

1.2 Process

The following steps were undertaken to produce this document:





**Neighbourhood Area
context analysis**

02

2. Neighbourhood Area context analysis

2.1 Location and area of study

The Pilning & Severn Beach Neighbourhood Plan (NP) Area (Map 01) covers an area of approximately 1535 hectares and is situated within the unitary authority of South Gloucestershire. Although part of the 'ceremonial county of Gloucestershire', South Gloucestershire is a local government unitary authority area in Gloucestershire. It is not administered as part of Gloucestershire, although historically it is part of that county.

The main settlements of Pilning & Severn Beach are accompanied by smaller hamlets including; Redwick, New Passage and Northwick. The Neighbourhood Area is located on the banks of the Severn Estuary, surrounding the M4 Prince Of Wales Bridge, which crosses the River Severn connecting England and Wales. Bristol City centre is located 13.5km to the southeast, and the county town of Gloucester 42km North.

The Neighbourhood Area is situated approximately 4.5km northwest of the M5 (J17) and 4km southwest of M48 (J1), with the local area serviced by the M4, M49, B4055 and A403. There is a rail-link at

Severn Beach which connects to Clifton Down and Temple Meads stations and a rail-link at Pilning which is on the main London Paddington to Swansea line, though there is very infrequent service to this station.

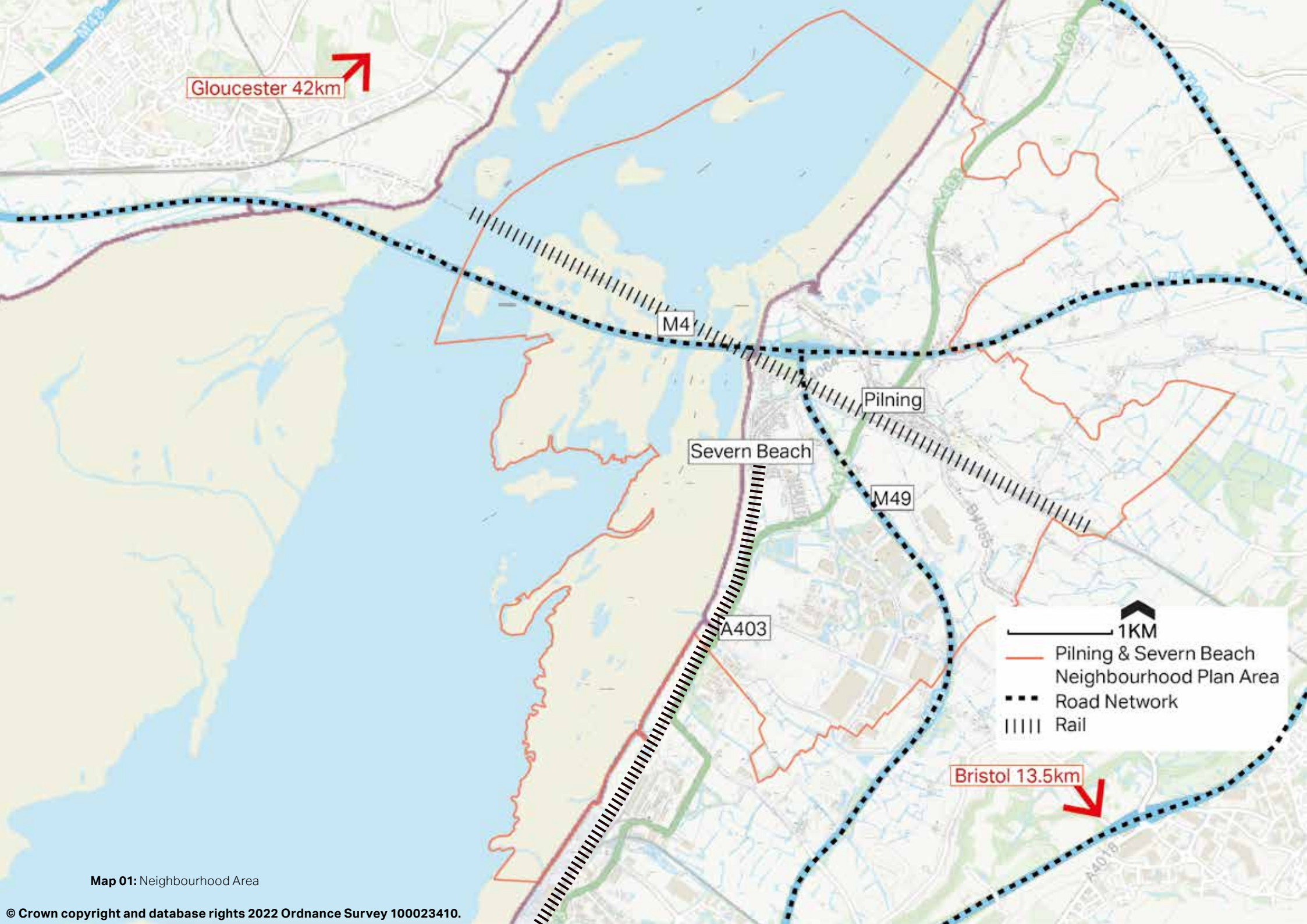
The Neighbourhood Area has amenities including churches, pubs, village shops (one with post office) and St. Peter's and Severn Beach Primary School.

The Neighbourhood Area falls wholly in the Severn and Avon Vales National Character Area (England) which is characterised by the "low-lying open agricultural vale landscape" with sparse woodland, widespread traditional orchards and "concentrations of lowland meadow" and floodplain marshland.

There is a good network of footpaths and bridleways across the Neighbourhood Area, which provide walking and cycling routes between the settlements and out into the surrounding countryside. The Severn Way, a long distance route which follows the River Severn from its source in Plynlimon, Wales to Bristol, also provides a walkway alongside the Severn Estuary and is part of the English Coastal Path.



Figure 01: Severn Way walking route alongside the Severn Estuary.



Gloucester 42km

M4

Pilning

Severn Beach

M49

A403

Bristol 13.5km

1KM

- Pilning & Severn Beach Neighbourhood Plan Area
- Road Network
- Rail

Map 01: Neighbourhood Area

2.2 Land use

A large portion of the Neighbourhood Area is comprised of the Severn Estuary and areas of rural open field land, made up of small pasture fields and commons primarily used for agriculture. There are sparse areas of woodland throughout the area and the landscape has an open and rural character.

The Severn Estuary historically was used for transport and trade. Today, up-stream ports have moved mostly to leisure uses, but down-stream ports are active and growing; Avonmouth has the prospect of establishing a full deep-sea container facility, bringing more trade into the region.

The M4 runs east-west through the centre of the Neighbourhood Area and the M49, which connects south to the M5, runs from the southern boundary of the Neighbourhood Area north to meet the M4 at Severn Beach. The M49 link road once completed, will allow access to the M4 bridge, known as the Prince of Wales Bridge, which provides access over the Severn Estuary into Wales.

Within the Neighbourhood Area there is a large area of commercial use land bordered by Severn Road and the railway line, south and east of Severn Beach. Commercial buildings in this area are used by companies including Amazon, Warburtons and Tesco. This area is subject to potential further commercial development due to 1957 extant planning consent on former ICI land.

The villages and hamlets of the Neighbourhood Area are primarily residential with some local amenities. Severn Beach has the most amenities with a convenience store, barber shop/ hairdressers, cafés, bakery, train station, primary school, church, village hall, public toilets, a few local businesses, allotments and park. The only doctor's surgery and pharmacy in the Neighbourhood Area is located in Pilning where there is also a primary school, church, village hall, pubs, restaurant, allotments and park.



Figure 02: View of open field land which covers a large part of the Neighbourhood Area.



Figure 03: View of the M4, Prince of Wales Bridge.

-
- Pilning & Severn Beach Neighbourhood Area
 - Vehicular access
 - Railway line
 - Railway station
 - Commercial area
 - Residential area
 - Open fields/ agriculture
 - Water/ wetlands
 - Solar farm

Map 02: Neighbourhood Plan Land Use

2.3 Landscape, ecology and heritage designations

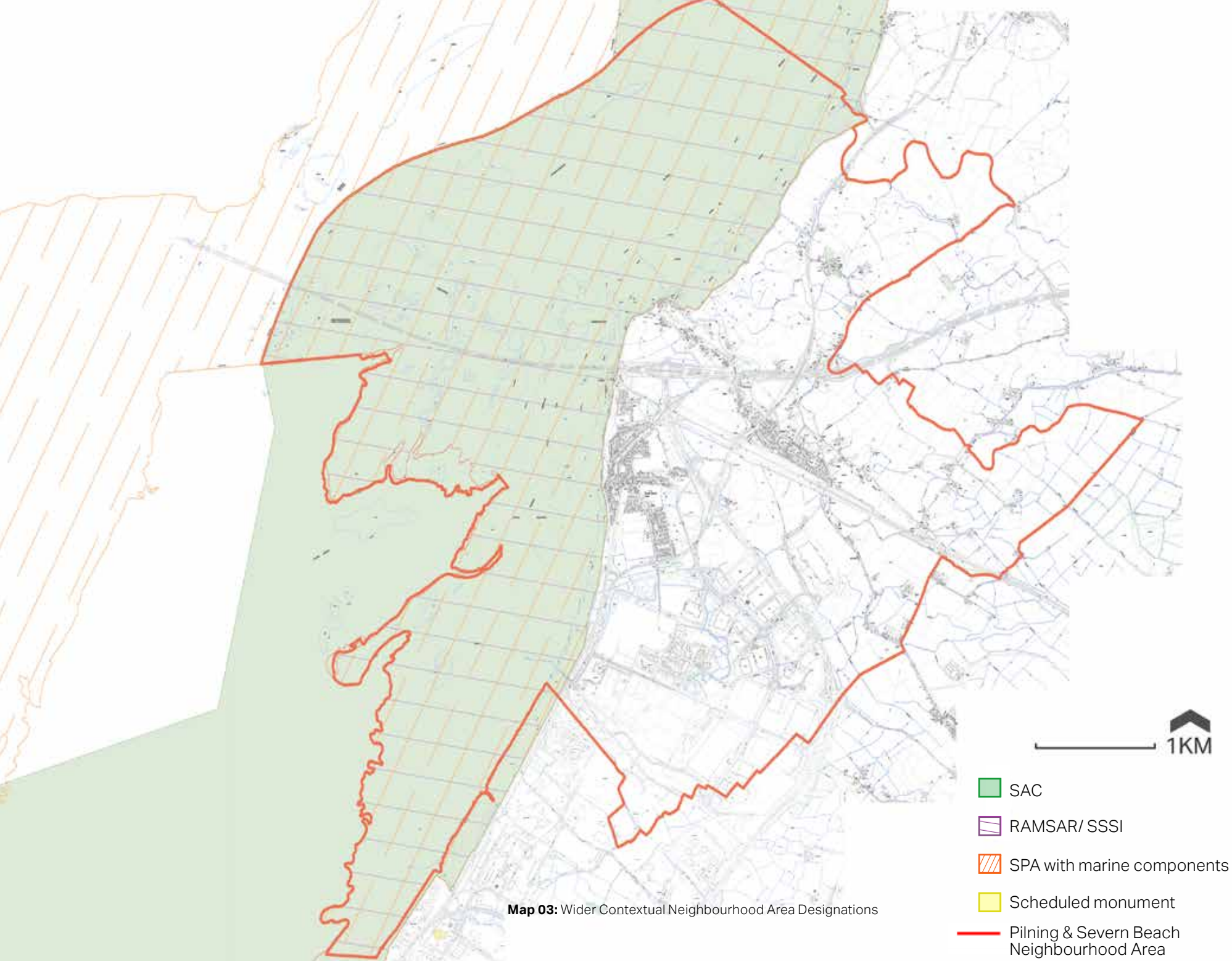
Within Neighbourhood Area:

- The Severn Estuary is designated as a Ramsar site, Site of Special Scientific Interest, Special Area of Conservation and Special Protection Area;
- There are a number of designated listed buildings and structures of mainly Grade II listed status. The only Grade II* listed structure is the Severn Tunnel East Portal (Ref. 1409175) which has engineering, historical and architectural value. Included in the Grade II listed buildings within the Neighbourhood Area are a number of dispersed farmhouses; the Parish Church of St Peter in Pilning and the remaining tower of the Church of St Thomas in Northwick; and
- There are several coastal priority habitat designations which cover parts of the Severn Estuary banks. These include stretches of coastal saltmarsh, small areas of maritime cliffs and large areas of slopes and mudflats. Much of the surrounding open field land of the Neighbourhood Area is also designated

as grassland priority habitat, classified as coastal and floodplain grazing marsh. There are additionally dispersed areas of deciduous woodland priority habitat, one area at Severn Beach, a few along the side of the M4 and one east of Pilning at Gumhurn Farm.

Outside the Neighbourhood Area:

- The nearest Scheduled Monuments are the Heavy Anti-aircraft battery 520m east of Holes Mouth (Ref. 1021291), which is located just to the south of the Neighbourhood Area boundary off the A403 and at Olveston, just to the east of boundary, is the Olveston Court moat complex and earthworks (Ref. 1012362); and
- There are several Local Nature Reserves near to the southern and eastern boundaries of the Neighbourhood Area. The closest is Lawrence Western in Hallen to the south of the boundary and Gorse Covert in Patchway to the east of the boundary.



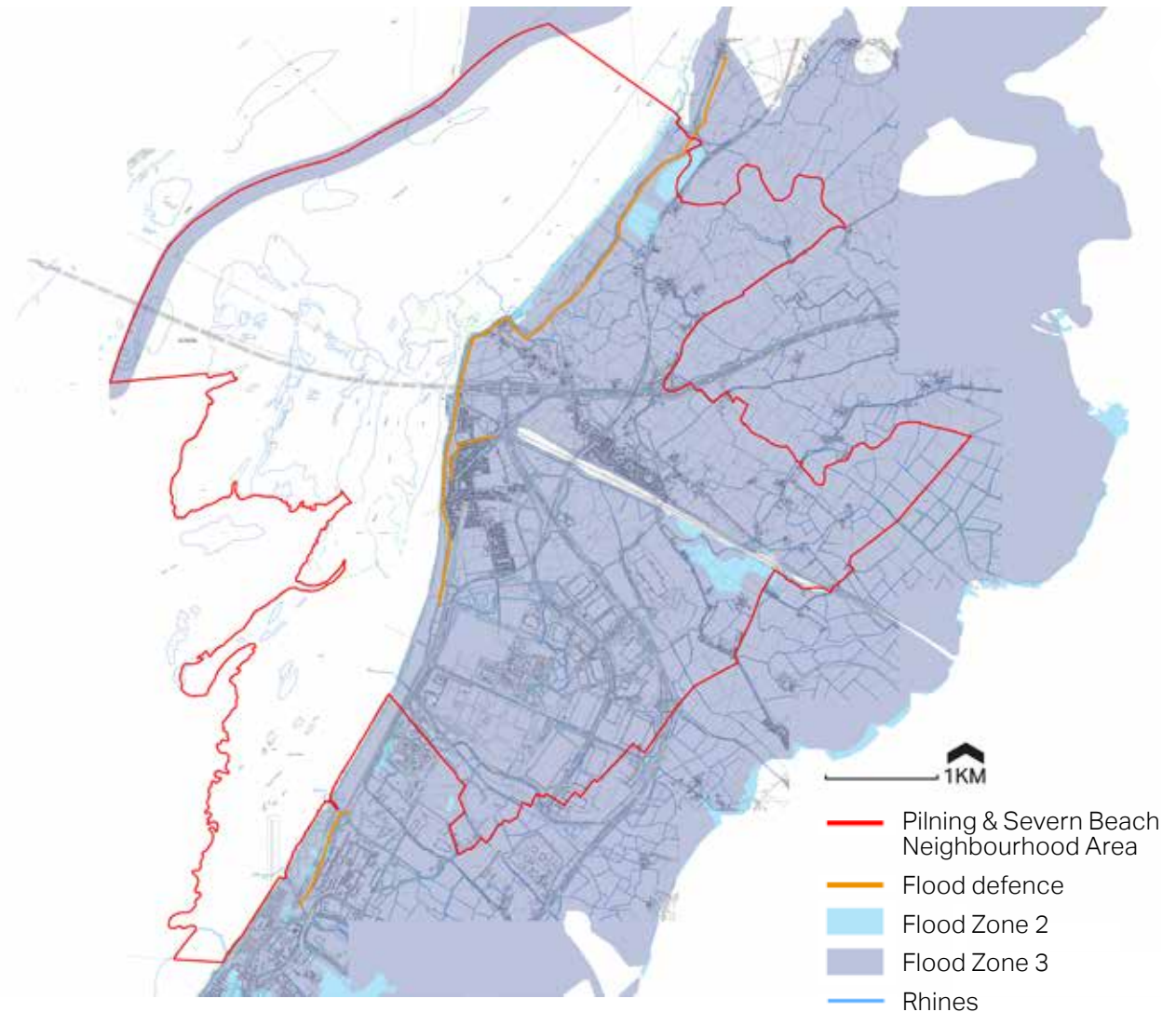
2.4 Water and flood risk

There is high flood risk throughout the Neighbourhood Area. The majority of the land is covered by Flood Zone 3 and there are small areas of Flood Zone 2.

Dark blue highlights the flood extent of Flood Zone 3 – 1% or higher chance of fluvial flooding and 0.5% or higher chance of tidal flooding in any year. Medium blue highlights the extended flood extents of Flood Zone 2 – 0.1-1% chance of fluvial flooding and 0.1-0.5% chance of tidal flooding in any year.

There are flood defence measures in place along this stretch of coastline from Avonmouth to Aust, which have recently been upgraded. In the area from Severn Beach to New Passage these are mainly in the form of precast flood defence walls.

Man-made ditches which follow field edges, known as rhines, aid drainage of the floodplain. The rhines drain into the estuary and therefore at times are subject to tidelock. These were originally constructed to reclaim marshland to use for agriculture and form an important characteristic and blue infrastructure element for the area.



2.5 Historic development

The parish is recorded on the Almondsbury and Henbury tithe maps of 1838-9. The landscape was mostly comprised of agricultural land, organised as a combination of rectilinear and more irregular-shaped fields, which separate a number of small settlements and farmsteads. The fields are recorded as being in use as a mix of arable, pasture and orchards. The settlements of Redwick and Northwick are labelled on the map. Both settlements were recorded as small collections of buildings concentrated along a main road.

By 1886 the landscape remained predominately rural with the land comprised mostly of agricultural fields. Some further land organisation also appears to have taken place by this time as a number of irregular-shaped fields on the tithe map appear more rectilinear in shape on the 1886 OS maps. There are also further additions recorded within the landscape. One such new feature is the Bristol and South Wales Union Railway. The railway line was completed in 1885 and ran across the River Severn, connecting England and Wales. Settlements showed some growth due to influx of ferry and railway workers, with the core of Pilning recorded including'



Figure 04: Henbury except Charlton tithe map, 1839

St Peter's Church, a Wesleyan chapel and a public house, located along the main road through the village. At the western side of the parish, on the edge of the River Severn an area of gravel banks is recorded, which also marks the location of a salmon pool, highlighting the fishing industry of the area at this time.

During the early 20th century, the settlements expanded, with additional buildings constructed in Pilning and in Severn Beach close to the river. The Avonmouth Branch of the Great Western Railway is also first recorded on the 1904 OS map.

The development of Severn Beach began in 1922 with the arrival of the railway and its development as a resort. Many new roads and buildings are recorded on the 1947 OS map. The settlement expanded throughout the later 20th century and saw the construction of a large business park at the southern end in the late 20th and early 21st centuries.



Figure 05: Gloucestershire Sheet LXVII.NW Six Inch Ordnance Survey map, 1886



Figure 06: Gloucestershire Sheet LXVII.NE Six Inch Ordnance Survey map, 1921

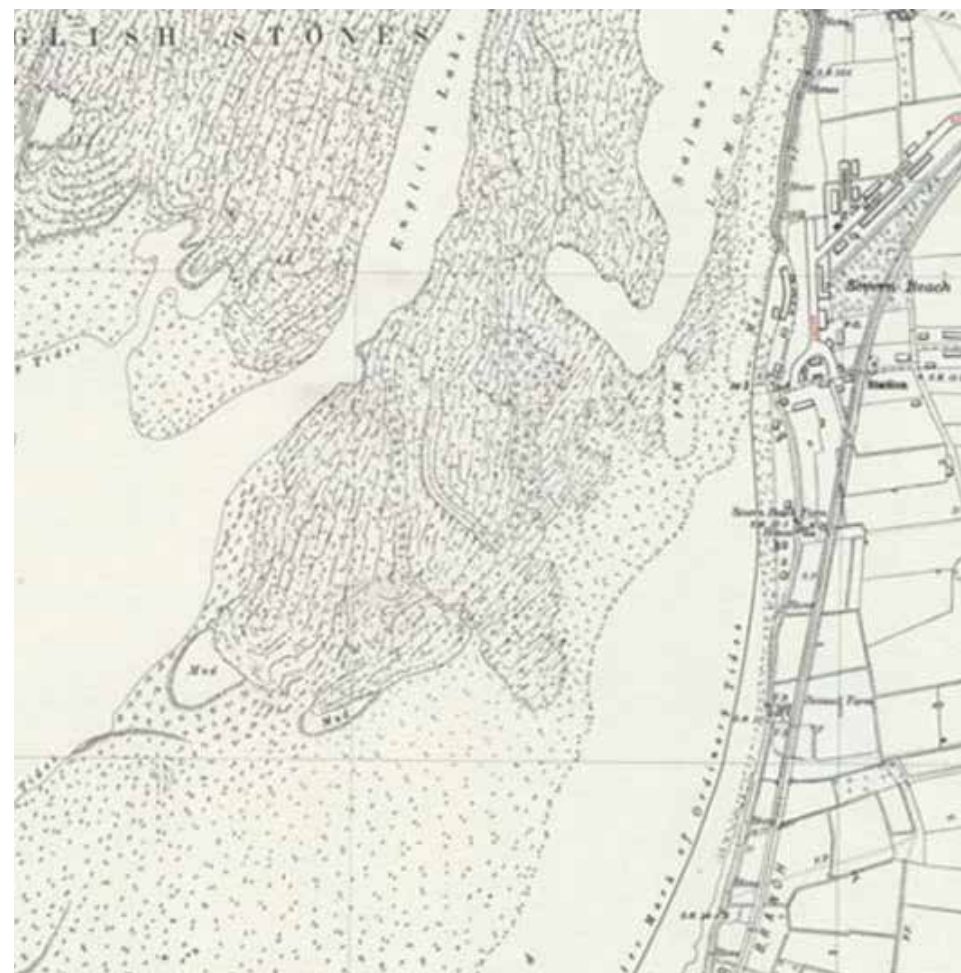


Figure 07: Gloucestershire Sheet LXVII.NW Six Inch Ordnance Survey map, 1947

2.5.1 Historical timeline

A historical development timeline has been produced to provide a summary of the historical development of the Pilning & Severn Beach Neighbourhood Area.

1086

Redwick recorded in the Domesday Book, which recorded 18 households and a mill within the settlement.

1247

First record of fishing in Severn estuary. Fishing methods included fish traps and weirs to catch salmon.



Figure 08: Putchers, Environment Agency.

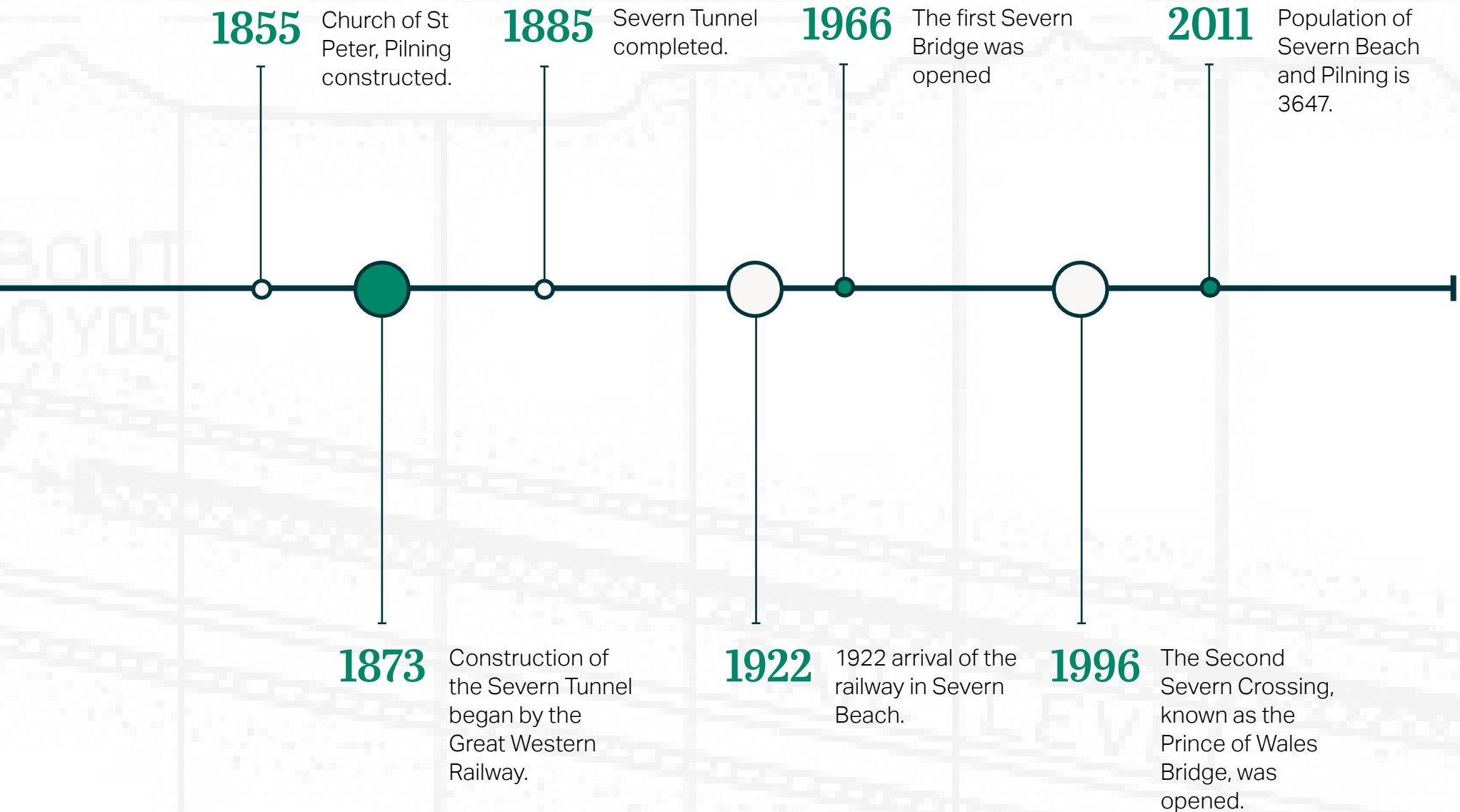
1131

First record of a regular ferry across the Severn between Aust to Beachley, used by monks at Tintern Abbey.

1718

The 'New Passage' ferry across the Severn opened to the south of the previous ferry route, near Severn Bridge.

RIVER SEVERN



2.6 Stakeholder engagement

Members of the Pilning & Severn Beach Neighbourhood Plan Steering Group were invited to share their knowledge and experience of the Neighbourhood Area during a site visit to discuss the stakeholders' requirements, key elements of settlement character areas and aspirations for the Neighbourhood Area.

Several key considerations and strategic issues emerged from the consultation, which have informed the preparation of the Design Code. These issues have been identified at a wider scale and represent the aspirations of the Pilning & Severn Beach Neighbourhood Plan Steering Group that can be achieved through design and masterplanning.

These are summarised below:

- Strategic parish location combining industry, marine and transport/logistical opportunities;
- Landmark infrastructure features;
- Opportunities and threats present in balancing economic land-use with residential/non-commercial land-use types, see Map 2;
- The influence of the Severn Estuary/ River context; and
- Attractive place to live and work with commuting benefits.

Pilning & Severn Beach statistics:*

- Pilning & Severn Beach Neighbourhood Area has a population density of 2.4 persons per hectare, indicative of the small-scale settlements and rural nature of the area;
- Semi-detached houses/ bungalows are the most common housing typology (34.6%) with detached houses/ bungalows also prevalent (33.5%) and terraced less common (17.6%);
- The mean age of the area is 39.8, and
- The majority of housing is under ownership (79.8%), private rented (13.8%) with social rented representing (5.1%).

**All information from the 2011 census.*

3. Character assessment

This section outlines the broad physical, historic and contextual characteristics of the Pilning & Severn Beach Neighbourhood Area.

3.1 Introduction

Character assessment is used to describe and articulate what is special and distinctive about a place. It is used to identify recognisable patterns of elements or characteristics that make one place different from another. This guidance is focused on the residential character of townscape and the landscape setting, informed by the work of the Neighbourhood Plan Steering Group and the site visit by the AECOM consultant. Non residential land use and temporary forms of accommodation such as park homes, of which there are two sites in Severn Beach, are beyond the scope of this assessment. Features introduced in this section are later used to inform the Design Codes and Guidelines.



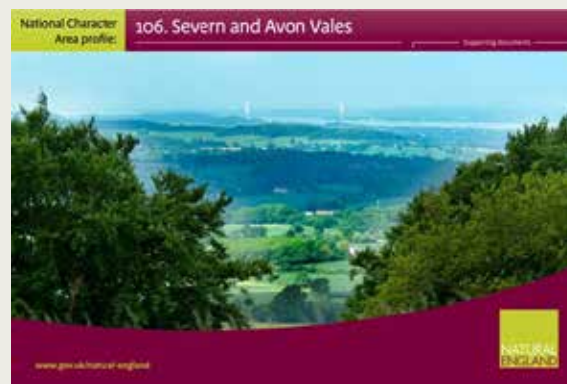
Figure 10: Red brick houses and the influence of the road network.

3.1.1 Existing character assessments and design guidance

The following National level published character assessments, management strategies and design guidance documents are relevant to the Pilning & Severn Beach Neighbourhood Area:

2014 National Character Assessment NCA Profile:106 Severn and Avon Vales (NE336)

NCA profiles are guidance documents which can help communities to inform their decision-making about the places that they live in and care for. The information they contain will support the planning of conservation initiatives at a landscape scale, inform the delivery of Nature Improvement Areas and encourage broader partnership working through Local Nature Partnerships



2021 - National Planning Policy Framework MHCLG

Development needs to consider national level planning policy guidance as set out in the National Planning Policy Framework (NPPF) and the National Planning Policy Guidance (NPPG). In particular, NPPF Chapter 12: Achieving well-designed places stresses the creation of high-quality buildings and places as being fundamental to what the planning and development process should achieve.



2019 - National Design Guide MHCLG

The National Design Guide (Ministry of Housing, Communities and Local Government, 2019) illustrates how well-designed places that are beautiful, enduring and successful can be achieved in practice.

2021 National Model Design Code MHCLG

Provides detailed guidance on the production of design codes, guides and policies to promote successful design. It expands on 10 characteristics of good design set out in the National Design Guide.

2020 - Building for a Healthy Life Homes England

Building for a Healthy Life (BHL) is the new (2020) name for Building for Life, the government-endorsed industry standard for well-designed homes and neighbourhoods. The new name reflects the crucial role that the built environment has in promoting wellbeing. The BHL toolkit sets out principles to help guide discussions on planning applications and to help local planning authorities to assess the quality of proposed (and completed) developments, but can also provide useful prompts and questions for planning applicants to consider during the different stages of the design process.

2020 - Living with Beauty MHCLG

This independent report introduces guidelines on how to promote and increase the use of high-quality design for new build homes and neighbourhoods.

2007 - Manual for Streets Department for Transport

Development is expected to respond positively to the Manual for Streets, the Government's guidance on how to design, construct, adopt and maintain new and existing residential streets. It promotes streets and wider development that avoid car dominated layouts but that do place the needs of pedestrians and cyclists first.



2013 - South Gloucestershire Core Strategy 2006-2027 South Gloucestershire Council

Adopted in December 2013 the South Gloucestershire Core Strategy sets out a vision for future development in South Gloucestershire up to 2027 and covers eight key issues developed alongside partner organisations and communities.

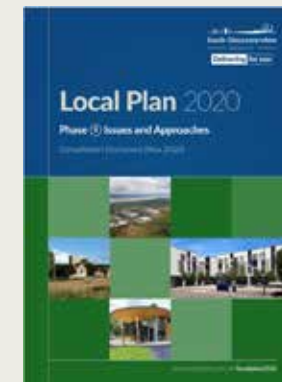
2017 South Gloucestershire Policies, Sites and Places Plan South Gloucestershire Council

Adopted in November 2017 the South Gloucestershire Policies, Sites and Places Plan

sets out policies for managing new development and for allocation and protection of sites.

2024 - South Gloucestershire Local Plan South Gloucestershire Council

Due to be adopted in 2024, it provides a planning policy framework for South Gloucestershire up to 2039 and will replace the previous 2006 Local Plan.



2014- Landscape Character Assessment South Gloucestershire Council

CA20 - Pilning Levels

CA21 - Severn Shoreline and Estuary

South Gloucestershire Council's Landscape Character Assessment provides an evidence base which explains the key characteristics which make each of the 21-character areas across the county distinct. It helps strengthen the ability of planning policy and development management decisions. It also protects and enhances the special qualities and features of South Gloucestershire. The relevant character areas for Pilning & Severn Beach are CA20 and CA21.



2022- Supplementary Planning Document (SPD) - Biodiversity and planning, Guidance for new developments

South Gloucestershire Council

The Environment Bill 2019-2021 will make a 10% Biodiversity Net Gain (BNG) a mandatory rule in planning. Developments must achieve this net gain within 30 years. The Biodiversity and planning SPD, currently in its draft stage, will provide information to support the Local Plan policies to ensure that biodiversity is adequately conserved, enhanced and achieves a measurable BNG throughout the development process.

For more information see <https://consultations.southglos.gov.uk/BioSPD>.

3.2 Character assessment

The character assessment is informed by the work conducted by the Neighbourhood Plan Steering Group and is structured around the main settlements and substructures of distinct character within the Pilning & Severn Beach Neighbourhood Area.

Listed below and illustrated on Map 05 are the six distinct areas identified by AECOM's character assessment. It should be noted, that these boundaries do not necessarily align with settlement boundaries and relate only to areas of urban design/architectural character:

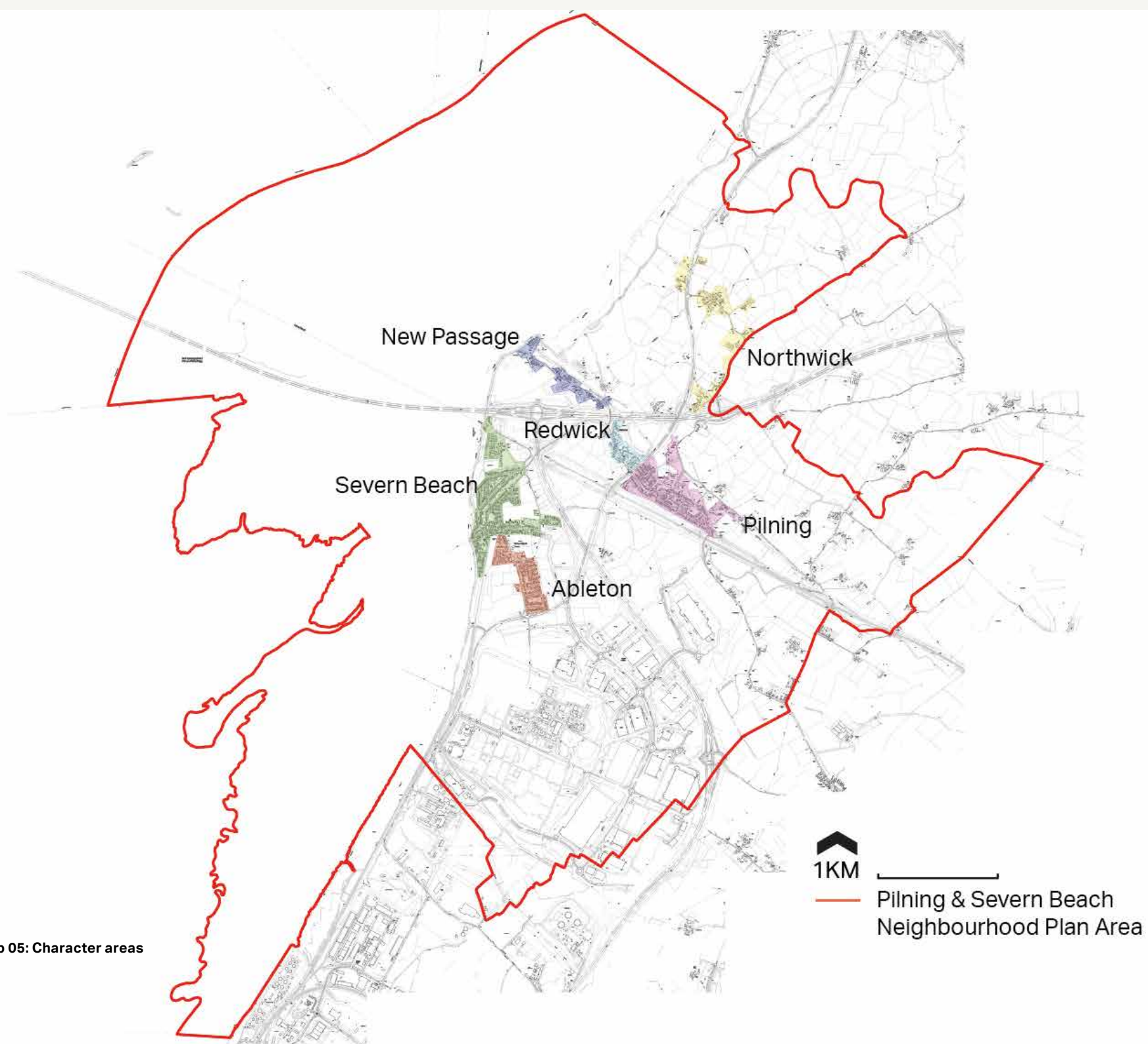
- **CA1 - Pilning;**
- **CA2 - Redwick;**
- **CA3 - New Passage;**
- **CA4 - Northwick;**
- **CA5 - Ableton; and**
- **CA6 - Severn Beach.**

The character assessment will cover:

- Pattern and layout of buildings;
- Access, green infrastructure, active travel and open space;
- Architecture and details;
- Materials;
- Building modifications, extension & conversion; and
- Sustainability and building performance.



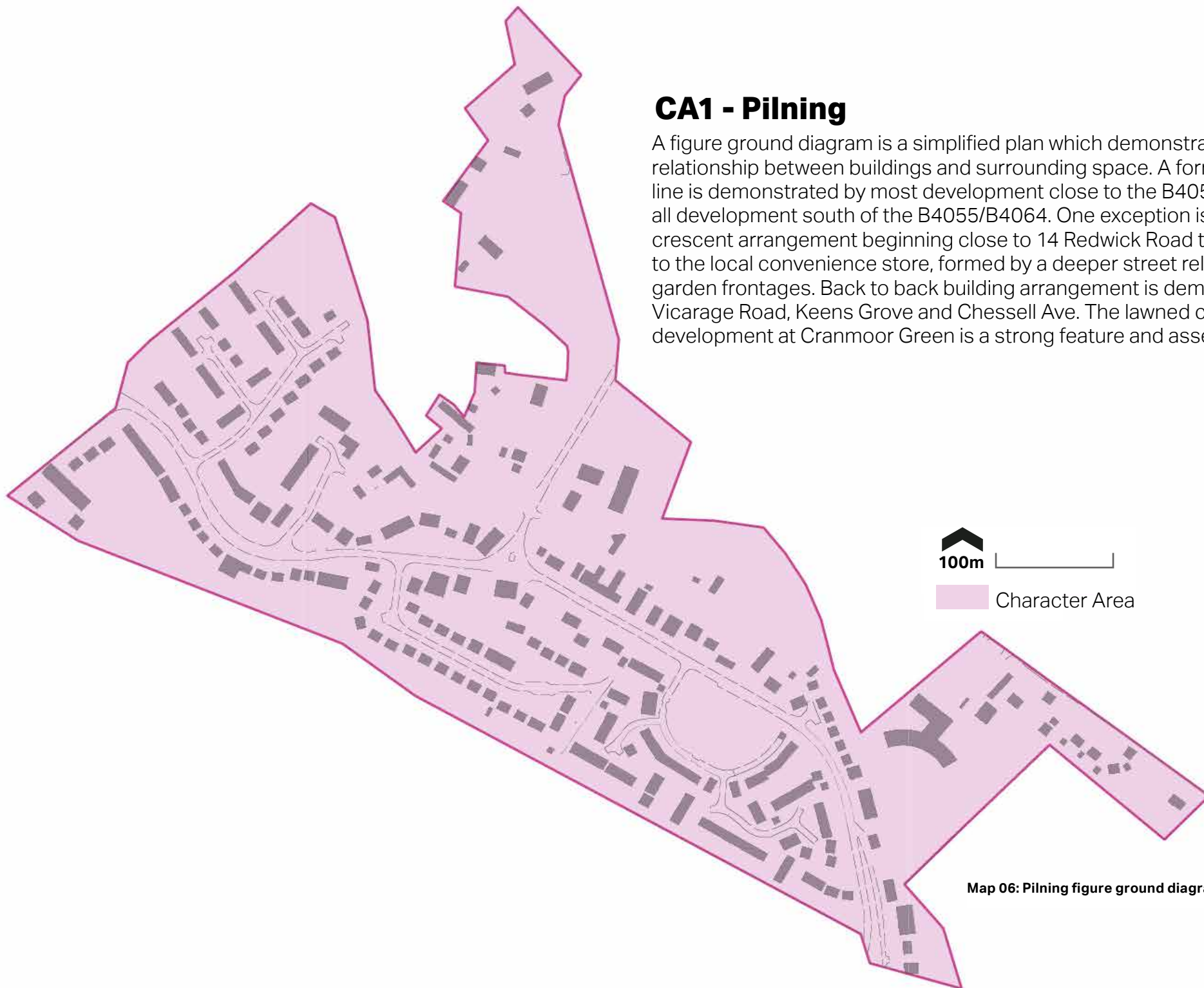
Figure 11: Salthouse Farm.



Map 05: Character areas

CA1 - Pilning

A figure ground diagram is a simplified plan which demonstrates the relationship between buildings and surrounding space. A formal building line is demonstrated by most development close to the B4055/B4064 and all development south of the B4055/B4064. One exception is the small crescent arrangement beginning close to 14 Redwick Road that continues to the local convenience store, formed by a deeper street relief used for garden frontages. Back to back building arrangement is demonstrated at Vicarage Road, Keens Grove and Chessell Ave. The lawned crescent fronting development at Cranmoor Green is a strong feature and asset.



Map 06: Pilning figure ground diagram



CA1 - Pilning

Pattern and layout of buildings

The character area of Pilning developed in the 19th and 20th centuries, concentrated around the historic core comprising 19th century buildings and a public house. The character area expanded throughout the mid-late 20th century further along the main road and to the north and south. The eastern and western ends of the character area are mostly defined by mid-late 20th century housing. The original settlement is linear in form; however the settlement's evolution now demonstrates nucleated characteristics, with later residential development contributing depth to the settlement structure albeit in cul-de-sac arrangements.

Buildings are arranged with primary facades facing the road and most include garden frontages and car parking. In the northern extents of the Character Area, The Pill watercourse contains (restricts) development plots, with some long rear plots existing. In the south, the railway line (in cutting) marks the development end. Beyond the Character Areas main roads (Redwick (B4064)/Cross Hands Road (B4055) and Northwick Road), there is an overuse of secondary road cul-de-sacs that do not loop back to the primary access.

Access, green infrastructure, active travel and open space

The A403 (Severn Road) is a busy 50mph dual carriageway imposing a peri-urban western edge to the Character Area, that can feel quite rural in places. The road also creates a feeling of severance from other areas of the Neighbourhood Plan Area. The main road B4064 /B4055 has been downgraded to a 20mph zone with speed bumps installed, indicative of speed related issues as the access could be used as a short-cut to the M5 at Cribbs Causeway. Overuse of secondary road cul-de-sacs that do not loop back to the primary access reduces settlement permeability, both vehicular and non-vehicular. There is pavement provision throughout the Character Area and two zebra-controlled crossing points. The road layout, War Memorial and trough mark the centre of the Character Area, combining with the convenience store and public house to emphasise the centre, and with no on-street parking restrictions, customers can park easily. There is a lack of bins and benches. Pilning train station is located approximately 800m east of the Character Area.

The lawned crescent fronting residential development at Cranmoor Green is an opportunity missed (in the original planning permission) for the community and would enhance the character area and the setting of the church opposite if this was boundary free.

Water courses and rhines are a feature of the area. Access to green space and green infrastructure is provided by: The green at Cranmoor Green, St Peter's Church & cemetery, allotments, playing field and St Mary's Indian Orthodox Church. There is no cycle provision, limited to non-segregated carriageway access only and a limited network of PRoW.

Views and landmarks	<p>Within central areas, views are contained by the relatively flat/constant topography and formal building line character. Road alignment provides some lengthy internal views, with trees, verges, and the contrasting materials of brick (red) and render (white) enhancing street interest. Northern extents of the Character Area (Bank Road) and again at St Peters provide opportunities for views across open farmland, and across the playing field at the latter. These parts of the Character Area offer the most opportunities for distant contextual views.</p>
Architecture and details	<p>Prominent buildings within the Character Area include the two storey Cross Hands Inn Public House comprising two storeys, rubble construction is likely with an exterior finish of rough cast render painted white with mock-Tudor timber framing to first floor. The building has a gabled double Roman tiled roof with two brick stacks. Trinity Hall, the former Wesleyan Chapel (locally listed), is constructed of red brick with ashlar dressings. The north front comprises a central entrance, with ashlar surrounds and buttresses to either side. Mafeking Hall, the former mission hall is located on the north side of Redwick Road. The building is constructed of snecked rubble with red brick dressings. The building is rectangular in plan with the entrance to the south-west elevation. The Grade II listed Church of St Peter (1137123) was constructed in 1855 in the Early English Gothic style, in snecked rubble with stone dressings and a plain tiled roof. The gate is also Grade II listed (1128951). Early housing examples within Pilning include Kings Cottage, Number 14 Redwick Road, a possibly 18th century locally listed building, comprising two storeys of roughcast render and a pantile roof with brick stacks and a central projecting pitched porch. Cranmoor Villa, a Grade II listed (1128950) early 19th century house is of rubble construction and rendered with a double Roman tiled roof. Residential building typologies are mainly detached or semi-detached and two stories with some bungalows. Roofs are commonly pitched (gabled or hipped) roofs, with noted variation throughout the Character Area. Terraces at Chessell Avenue, Keens Grove, The Glebe and Redwick Road are formed in short rows. Construction type includes solid stone walls, and more recent cavity (masonry/brick) wall. Render is common and there is some use of tile hanging. Decorative brick pattern embellishment and barge boards can be seen at Cranmoor Green. There are examples of timber and white uPVC windows and doors. Redwick Road is particularly eclectic, combining old cottages mainly at the eastern end, a former modern police station, vicarage and steep roofed storey and a half dwellings.</p>



Figure 12: - Stone cottage, gable upstands and double Roman



Figure 13: - Grade II listed War Memorial (1426987), located in the centre of the junction with Northwick Road



Figure 14: - Character area centre convenience store and on-street parking

Materials	<p>Earlier building stone construction is in limestone and sandstone, owing to the area's Triassic mudstone, siltstone and sandstone formations and nearby quarries at Almondsbury and Olveston where carboniferous limestone has been quarried. Stone use is specified mainly as random rubble albeit there are examples using cut stone or ashlar embellishments. Brick is a material staple in the Character Area and following the completion of the tunnel, Cattybrook would have been a likely supplier for development here. The Character Area's vernacular roof covering is double Roman clay tiles. Quality local material examples include Trinity Hall with the large stone mullioned windows, buttresses to either side, ashlar pinnacles above and a central cross finial to the gable. And Mafeking Hall, with snecked rubble and red brick dressings, red brick surrounds to windows and door and decorative wooden bargeboards at the gables. Elsewhere, material evolution is evident across the Character Area, however there is a general erosion of local character through the loose specification of building materials. Some properties on Cranmoor Green, are specified with red double Roman tiles on dwellings constructed with orange brick stock producing a clash.</p>
Building modifications, extension & conversion	<p>Across the Character Area there are examples of small-scale adaptations, such as pitched porches and larger projects such as extensions. There are some examples of poor maintenance where buildings degrade the appearance of the Character Area. Cranmoor Green has aged well (25 years) by combination of material specification and user maintenance. Recent construction tends to favour brick and render, and therefore there is an associated risk stone construction will diminish.</p>
Sustainability and building performance, waste, recycling and utilities	<p>Earlier solid stone wall development, especially detached dwellings, with more exposed elevations will have lowest building energy performance. The lack of terraced typologies also impacts energy performance. Cranmoor Green is an example of recent residential development built to updated building regulations, however the development contains mostly detached dwellings. There are few examples of properties with PV and the Character Area has a high dependency on oil for space heating.</p>



Figure 15: - Green space with SuDS water management features.



Figure 16: - Erosion of boundaries and front gardens used for car parking. Pedestrian user on pavement.



Figure 17: - Storey and a half housing, roof lower at front facade.



Figure 18: - Orange toned brick with red double Roman tiles.



Figure 19: - Grand facing gables with lower storey bays.



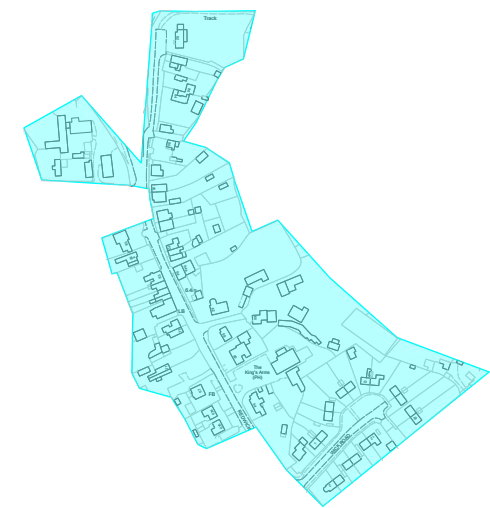
Figure 20: - Short row terraces, Keens Grove.

CA2 - Redwick

A figure ground diagram is a simplified plan to demonstrate the relationship between buildings and the surrounding space. In southern areas building line is informal with minimal spatial relationship between separate developments. From number 73 heading north, building line becomes more formal, albeit with some variation in offset, such as at 83a (The White House) which strikes some unity with 67a in the south. The arrangement of Wick Road does not bookend settlement development and present positively towards the A403. From the north, the transition from the green buffer (M4) into the settlement with meandering road has a rural quality.



Map 07: Redwick figure ground diagram



CA2 - Redwick

Pattern and layout of buildings	<p>The character area of Redwick is concentrated along Redwick Road, running north-west to south-east. First separated from Piling with the construction of the A403 and later separated from New Passage by the development of the Second Severn crossing supporting the M4 motorway, Redwick 'sub-village' is formed by circa 40+ dwellings of mixed design. Several of the earlier buildings are post-medieval in date. The Character Area is derived from a linear arrangement; however variance is introduced with the inclusion of farms (central north & north west) arranged in a non-linear formation, and with a greater offset from the main access (Redwick Road). Wick Road is set out as a cul-de-sac (secondary road) without onward connectivity, and easterly extents do not maintain the building line along the A403, subsequently the edge to the Character Area is vague. Further, the public house arrangement and residential housing opposite, both have greater set back from the access and combine to provide further variation to the linear arrangement. The latter residential arrangement influenced by location of a culverted and open section water channel. A mixture of two storey typologies are arranged with primary elevations facing the access road. Building line is generally informal relating to the variance in offset and arrangement.</p>
Access, green infrastructure, active travel and open space	<p>The character area spans the gap between the M4 and A403, terminating north west at a roundabout with onward options; north over the M4 on New Passage Road, or south west on Green Lane. The presence of the M4 means there is well established green buffer at this end of the settlement. Development along Redwick Road (north and south) back onto agricultural land with field boundaries. Central pavement provision is continuous on the northern side of Redwick Road. There is no publicly accessibly green space within the Character Area, however a large verge with tree planting (south) at the junction with A403 is an asset. A dual use access provision passes along Green Lane on to New Passage Road providing north south connections for PRoW and National Cycle Network Route 4.</p>
Views and landmarks	<p>Character Area views are limited to short linear internal views along Redwick Road. Individual properties may benefit from views over agricultural land, especially from upper floors. Local landmarks include examples of post-medieval buildings including The King's Arms Inn and the historic stone bridge on the nearby PRoW network.</p>
Architecture and details	<p>The architecture of the Character Area is simple and the area has an agricultural/worker feeling attributable to the simple short row terraces and glimpsed views through to farms/paddocks in behind. Dwellings are mainly two storey with pitched roofs, often with gabled or mono-pitched porches. Window fenestration is sometimes unbalanced and economical. The King's Arms Inn is a post-medieval public house recorded on the 19th century OS maps and the Henbury tithe map, 1839. The building is rendered with a gabled double Roman tile roof.</p>

<p>Architecture and details</p>	<p>There are some examples of embellishment which goes against the general simple approach. Early examples include Number 73 and 75 Redwick Road, comprising two semi-detached 19th century houses of red brick and double Roman tile roof. Number 73 has exposed red brick to the east front with two bays of segmental arch windows with ashlar lintels and keystones. Number 75 is rendered to the east front with a central entrance and segmental arch windows with red brick lintels. A leaded dormer windows form, mirrors the lintel embellishment to the garage below. Another late 19th century house is the former butcher's shop at Number 54 Redwick Road. Located towards the southern end of the Character Area, the building is built similarly to others in the village, with rendered walls, tile roof and brick stack. The house comprises two storeys and four bays to the south-west front with bay windows to either end. The house is surrounded by a small lawn which is bordered by a low stone and brick wall.</p>
<p>Materials</p>	<p>Early 20th century terraced housing on the west side of Redwick Road, comprising two storeys and two bay rendered houses with double Roman tile roofs and brick chimney stacks. Smooth and rough cast render is painted white or cream with some left unpainted, and the colour of the sand used.</p> <p>On the eastern side of the road larger houses are of similar construction, with rendered walls, tile roofs and brick stacks. The southern end of the Character Area contains mid-late 20th century housing along Wick Road. The houses are semi-detached two storey brick buildings constructed using a palette of 4/5 brick tones with a unifying brown concrete roof tile in double Roman style and brick chimney stacks. Some houses have tiled monopitch porches. A later corner infill property introduces render, red brick and red double Roman tiles. Externally some crating has been applied to grass verges to protect grass from car parking damage. There are examples of snecked boundaries, some with oval cappings, mortared stone-faced walls, blockwork walls, metal railings, panel fence and hedge boundaries.</p>



Figure 21: - Simple terraces, with economical window use.



Figure 22: - The Kings Arms Pub with car park and deep road offset that will accommodate circa. 25 cars.



Figure 23: - Parapet roof rendered property with decorative brick stacks.

Building modifications, extension & conversion	<p>There are some examples of plot splitting and infill development and likely adaptation/conversion of farm buildings, a process by which smaller less invasive incremental settlement changes are made. Outbuildings which tend to be a legacy of agricultural properties can be useful, especially with more widespread working from home practices.</p>
Sustainability and building performance, waste, recycling and utilities	<p>Older terraced typologies will have improved building performance compared with older detached dwellings, both present within the Character Area. Many buildings have chimneys and likely use open fires/stove to supplement the heating strategy. Chimneys are omitted on 67, 67A & 67B.</p> <p>Wheelie bin usage is difficult for terraced typologies and central areas are susceptible to bins being left out on the street. Power is distributed overhead via poles along with telephone services. A number of properties have PV installations the Character Area has a high dependency on oil for space heating.</p>



Figure 24: - Dwellings 'L' form wraps to create nook for porch. Good precedent detail for an end-terrace.

Prepared for: Pilning & Severn Beach Neighbourhood Plan



Figure 26: - Pitched property with potentially problematic large flat roof valley and twin PV array.



Figure 27: - Brick tones at Wick Road.



Figure 28: - Stone faced cottage.



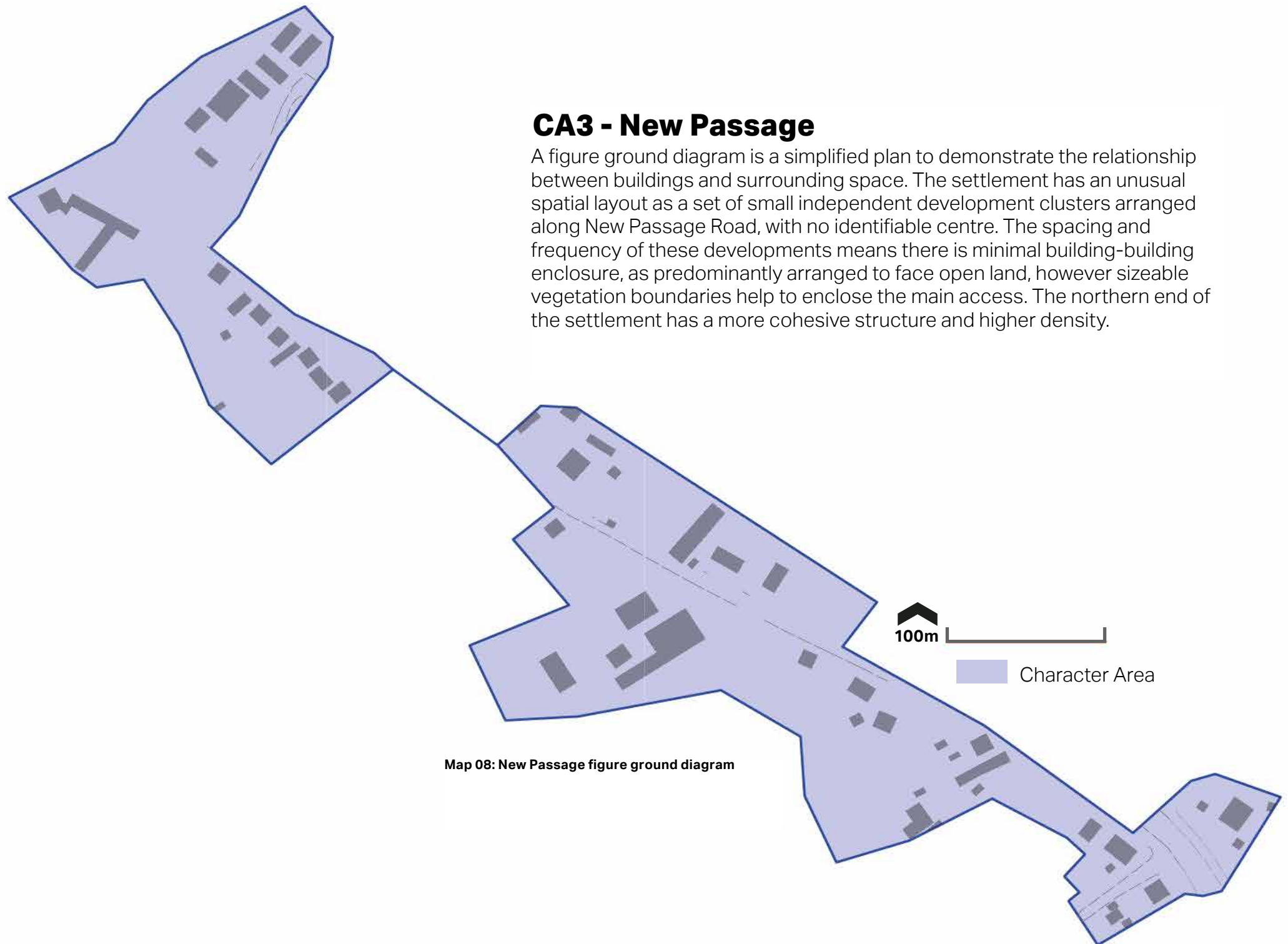
Figure 29: - High quality window and surround detail.



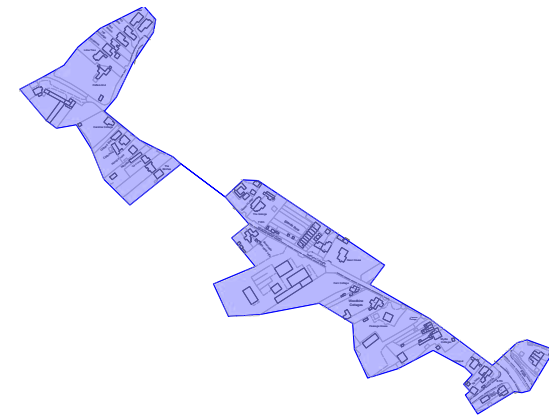
Figure 30: - Redwick southern edge at A403

CA3 - New Passage

A figure ground diagram is a simplified plan to demonstrate the relationship between buildings and surrounding space. The settlement has an unusual spatial layout as a set of small independent development clusters arranged along New Passage Road, with no identifiable centre. The spacing and frequency of these developments means there is minimal building-building enclosure, as predominantly arranged to face open land, however sizeable vegetation boundaries help to enclose the main access. The northern end of the settlement has a more cohesive structure and higher density.



Map 08: New Passage figure ground diagram



CA3 - New Passage

Pattern and layout of buildings	<p>New Passage is a small settlement subjected to severance following the construction of the M4. It is bounded by the River Severn and flood defence (north & north east) recently which was upgraded and raised to +10.4m CD to improve protection for the area which includes farmland, light commercial, water treatment an ex-MoD property and residential properties, some of which are below high tide water level. The linear settlement structure is unusual, arranged as separate development areas, with development breaks in-between. An area of bungalow development in the north demonstrates a more typical arrangement with buildings arranged behind generous front gardens opening onto the main access. A further sub area branches perpendicular off the main access in the north, with approximately nine dwellings arranged to capture estuary views. Blands Row (central) is a striking perpendicular terrace, originally built for use by ferry operators. North of New Passage Road, the sewage works and dismantled railway line are located.</p>
Access, green infrastructure, active travel and open space	<p>The Character Area integrates and borders areas of green and blue infrastructure however there is no publicly accessible green space, beside a large, lawned verge in the south with two trees. Severed in the south by M4, the low-density character area is surrounded by agricultural land and the River Severn Estuary, with parts designated as SSSI, SPA, SAC (Marine) & Ramsar. Main access is provided by the B4064 (New Passage Road) which terminates in the north. Lanes are not marked, and the speed limit is 30mph. There is no bus service. A small network of Public Right of Way (PRoW) serves the area which provide access from Chessell Pill along the shoreline and under the Prince of Wales Bridge and to the B4064. Non-vehicular access at the River Severn Estuary provides an important and accessible area, popular with walkers, dogwalkers, school children, cyclists, visitors and residents. Access onto cycle route 4 is possible in the south of the settlement, providing onward connections and route options north and south.</p>
Views and landmarks	<p>Settlement views are possible along the main access with opportunities for longer views in the south towards hill forms in Monmouthshire. Expansive estuarine views at the River Severn Estuary are possible (north, west & south) encompassing Caldicot, areas of Wales, wetlands, the Prince of Wales Bridge (M4), M48, Wye Bridges and Chessell Pill. There is interpretation signage located here also. A remaining outcrop of the 150m long railway pier (New Passage Pier) built prior to the Severn Tunnel and bridges is visible. Severn Lodge Farmhouse is a prominent building locally with gambrel roof and many chimney pots. Further, the New Passage heritage trail highlights elements of importance with signage. A Queen Victoria (VR) post box is located at the northwest end of New Passage, set into a stone wall.</p>

Architecture and details

The earliest buildings are late 18th and 19th century in date and comprise cottages, farmhouses, a terrace, and a chapel. The Grade II listed Severn Lodge Farmhouse (1137201) is located at the far north-west end of the Character Area and was built in the late 18th century with later additions. It has a notable slate gambrel roof with raised coped verges and gable stacks. There is a wrought iron canopied balcony with leaded roof along the first-floor frontage. Caroline Cottage formerly used as a shop, is located on the south-west side of New Passage Road. The building has two storeys, there is a central projected gabled porch with a plain tile roof canted bay windows to either side. Woodbine Cottages are another late 19th century addition, located towards the south-eastern end of the Character Area. Dated 1880 and consists of a pair of semi-detached houses, with central projected rubble porch with a hipped slate roof. Bland's Row, a row of 19th century cottages, is located centrally aligned perpendicular to New Passage Road. Each cottage is L-shaped in plan with a projecting bay to the south-east. There are several low quality flat roof additions. Queen's Lodge and Caledonia, a pair of large semi-detached houses comprise two storeys, rendered with ashlar dressings and slate hipped roof. At the southern end of the character area is the former Zion chapel, dated 1879 with a central projected gabled porch and a round arched door, either side there are tall round arched windows and above the porch is a large stained-glass oculus with date stone.

There is a concentration of 20th century housing towards the north of the Character Area. Comprising a row of bungalows set back from road with parking area and garden frontages. Style and materials vary consisting of a double pitched bay fronted dwellings with inset porch, converted upper storey and rooflights. An example with a large central vertical chimney structure. A brick bungalow with integrated garage, pitched roof and flat roof dormer. A pair of gable facing bungalows, one heavily textured (Tyrolean) and separate garage building the other finished in brick, with front feature reconstituted stone panel. Backing onto the estuary, an area formerly used by the New Passage Hotel, built in the 19th century close to the railway line, but demolished in the 1970s with new housing built on the site. The house at the southern end of the row is a mid-20th century modernist house of brick with partial cladding. The house is built into the former railway embankment and consists of one storey to the northern end and two storeys to the south. Gable upstands are seen elsewhere in the south also. Further along there is a contemporary pair with offset gable clerestories and at the end two large roof dwellings with diminishing gable and over-garage accommodation.



Figure 31: - Severn Lodge with raised coped verges



Figure 32: - Composition of flat and monopitched roofs and facades in render, standing seam and composite cladding. generous window openings



Figure 33: - Deane House

Materials

The area demonstrates material evolution with settlement development spanning the 18th, 19th, 20th and 21st centuries. Earlier construction examples include rubble and stone, some with elevation (rubble) rendered. There are also examples of low stone walls with cut stone cappings and brick piers or stone walls with metal railings. For example, Severn Lodge Farmhouse built in the late 18th century with later additions is constructed of stone rubble and rendered with stone dressings. Caroline Cottage dated 1870 is rendered with a pantile roof tile roof and rendered gable stacks. The chapel is constructed of snecked rubble with red brick dressings and Blands terrace are rendered with hipped tile roofs, most with modern casement windows. Throughout, brick embellishment at quoin and lintel is common. Older buildings are unified using red clay double Roman or pantile roof tiles, some with a blackening patina and most with brick chimney stacks. Passage house is roofed in slate. Woodbine cottage has an open dual porch with dwarf stone wall and chunky timber structure. Some 20th century dwellings lack material vernacular synergy. The specification of corrugated concrete tiles, buff brick, reconstituted stone or panel fencing at frontage for example, goes against the area's character, with the variation not pursuant of improved quality, but merely loose specification choice. There are some good examples of 21st century development with quality specification. A timber clad house behind Passage House is well detailed including a garage with an ecological roof and a recent outbuilding conversion/newbuild at Severn Lodge with weathered timber cladding, red brick plinth and pantile roof. Windows and doors are presented in brown/wood, black/grey and white colour uPVC and there are some high quality examples of timber sash.



Figure 34: - Severn Way and Heritage Trail signage.



Figure 35: - Interpretation signage, River Severn, bridge crossing and distant views of Wales.



Figure 36: - Bland's Row main access at rear. Arrangement perpendicular to New Passage Road.

Building modifications, extension & conversion

Examples of modifications and building conversion mainly relate to older building stock, with further evidence of plot splitting in the north east along the estuary frontage and an example at Passage House. Passage House is a large house with standalone garage and loft conversion. Bland's Row appear to have been adapted and expanded over the years and in filled between previous extensions, some specification includes flat roofs. The enviable estuary frontage has seen quite a lot of modification and updating, interestingly with housing typologies grouped in pairs, and all with integrated balconies designed to maximise views toward the estuary.

Sustainability and building performance, waste, recycling and utilities

The area contains several heritage dwellings that have been well maintained and subject update. Internal update will likely have included upgrade in building performance terms with internal insulation, care in detailing is crucial to manage interstitial condensation risk. Window replacement has occurred across the Character Area, which although presents uplift in building performance, must be balanced with building character. Bland's Row a short terrace of 8 dwellings which will benefit thermally from the terrace arrangement. Later extensions at Bland's Row potentially built to a higher performance will provide some shielding to older walls by infilling areas. At the estuary frontage, some examples constructed in the 21st century will benefit from upgraded construction and better performing components such as windows and doors. Generous window sizing and expansive open views will undoubtedly offer potential for passive gains, albeit these façades will be subjected to heightened weather exposure (potentially saline) and treatments should be specified to provide longevity and resilience. Another recent 21st property has specified sedum ecological roof on the garage, which will improve biodiversity habitat and slow the passage of rainwater. The Character Area has a high reliance on oil for space heating and there are some examples of PV.



Figure 37: - A pair with offset gable clerestories.



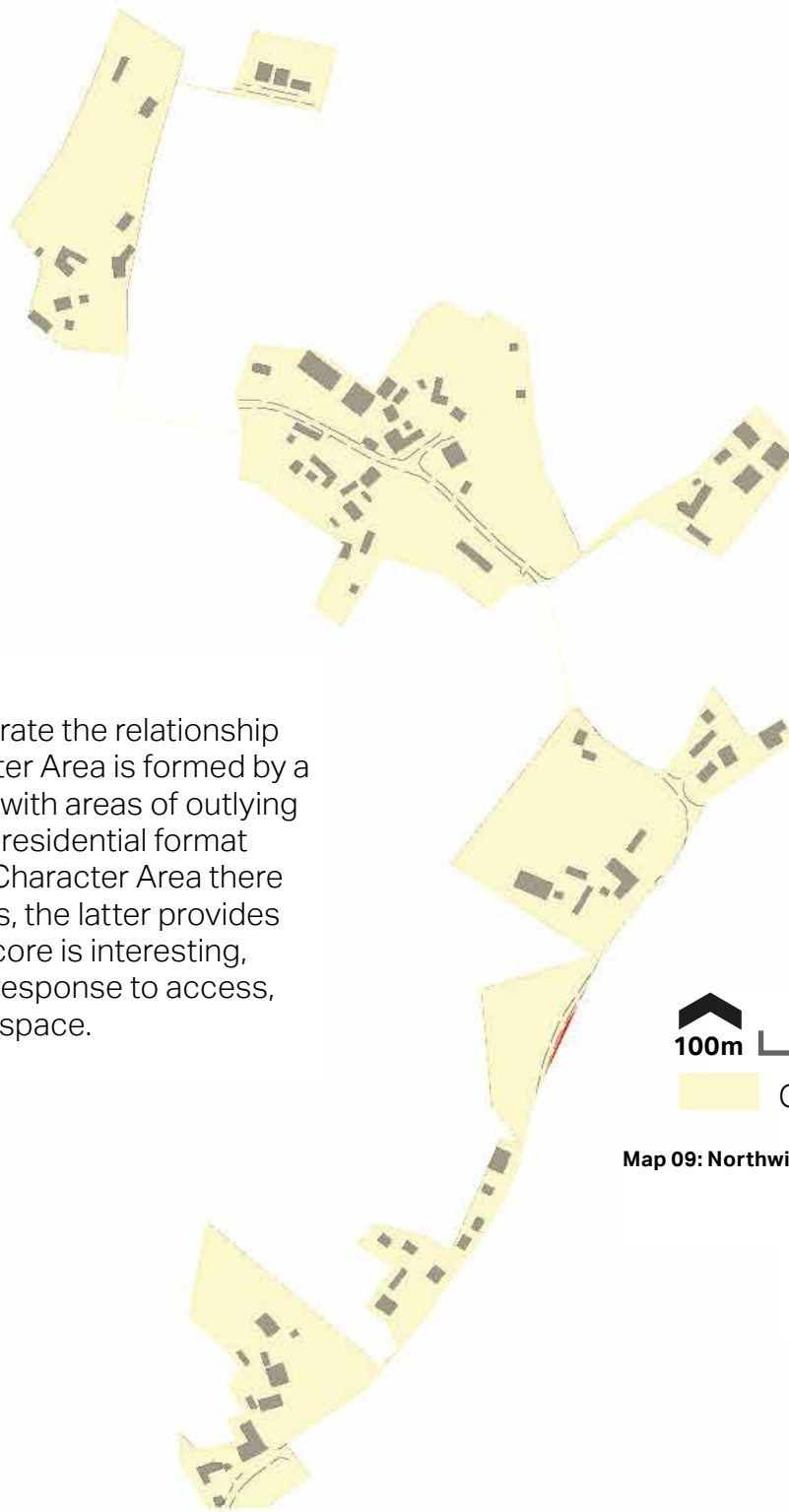
Figure 39: - Simple in keeping outbuilding conversion/new build.



Figure 38: - Caroline Cottage, rendered with gable

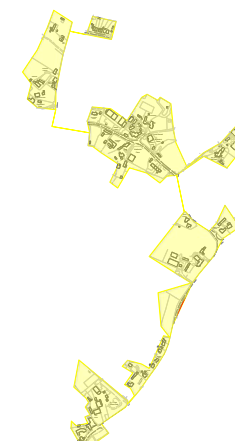
CA4 - Northwick

A figure ground diagram is a simplified plan to demonstrate the relationship between buildings and surrounding space. The Character Area is formed by a core of historic farm, church, and school development, with areas of outlying ribbon development structured in a more recognisable residential format aligned to face primary access roads. Throughout the Character Area there is the bucolic influence of farmland and field boundaries, the latter provides enclosure to Northwick Road. The arrangement of the core is interesting, mainly with buildings facing east or west, potentially in response to access, facing the church, or to maximise useable external sun space.



Character Area

Map 09: Northwick figure ground diagram



CA4 - Northwick

Pattern and layout of buildings	<p>Northwick is a rural character area mainly concentrated along Northwick Road (B4055), the character area centre and main nucleus of development is beside the former St Thomas's Church one-bell square tower, the main church declared structurally unsafe in 1962 and demolished. The nucleus is formed by 3 legacy farms Church Farm, Northwick Farm and slightly to the east – Mill Farm. The character area contains Grade II listed post-medieval buildings mostly, the earliest extant residential buildings include Church Farmhouse (1128952) and Mill Farmhouse (1128954), both 17th century farmhouses with later additions. Farmhouses align facing east/west with main facades fronting onto access that runs north/south perpendicular from the B4055. At the junction Bilsham Lane, another small nucleus of development exists comprising Grade II listed Northwick Farmhouse (1312459), Pound Cottage and Manor Farm. Further south housing is formed in more recognisable residential building plots front the road access facing east, Northwick Hall is also located here with a greater offset from the main access. The White Horse (public house closed since 2020) is located off a spur in an area with mixed commercial and residential land use.</p>
Access, green infrastructure, active travel and open space	<p>Access is provided by Northwick Road (B4055) (30mph speed limit), there is no pavement provision but some areas of enlarged grass verges. The Character Area has a useful PRoW network, the route for National Cycle Network Route 41 and National Cycle Network Route 4 pass through the Character Area. There is no public green space except the cemetery, but the surrounding agricultural landscape integrates field margins and there is a substantial green infrastructure buffer to the M4 in the south. A network of rhines and drains provide a blue infrastructure asset also. The area is served by the school bus.</p>
Views and landmarks	<p>The Character Area is quite uniformly flat and this characteristic combined with roadside tree and field boundaries can provide enclosure and reduce the number of possible viewpoints. Breaks or views over lower boundaries offer views across agricultural land and toward associated buildings. Internal views along Northwick Road (B4055) are possible, some of which include views towards local landmarks, including Church Farmhouse and the one-bell square tower. The spatial arrangement here of Church Farmhouse facing a junction, beside a green area with mature trees, the old school wall and the old church tower contribute to the central 'setting' and main core of the Character Area.</p>

Architecture and details

Northwick is a rural character area concentrated along Northwick Road. The character area contains a number of Grade II listed post-medieval buildings mostly concentrated within the centre of the village. The earliest extant buildings include Church Farmhouse (1128952) and Mill Farmhouse (1128954), both 17th century farmhouses with later additions. Church Farmhouse is situated in the centre of the village, constructed of rubble to the right and brick to the left, rendered at the front and sides. It has a pantile roof with gable stacks, and a double Roman tiled attached stable wing to the right. The farmhouse consists of two storeys and three windows, all sashes, the second from left has gabled porch with bargeboards and a finial. The east elevation has a two-light casement at attic level. The farmhouse is surrounded by a garden and bordered by a low rubble wall. Mill Farmhouse is located towards the eastern end of the village. It is constructed of rubble and partly rendered with a pantile roof with gable stack and ridge stack. The former front, now rear, to south-east consists of 1½ storeys. It has a plank and batten door in a heavy frame with a timber lintel and 20th century porch. The windows are all 20th century, the ground floor with timber lintels. There are also two early 19th century Grade II listed houses within the centre of the village: Northwick Farmhouse (1128955) and Northwick Lodge (1137171). Northwick Farmhouse is constructed of rubble and rendered, with a pantile roof and gable stacks. The farmhouse consists of two storeys and three window range. The first floor has a central boxed sash and all others are 16-pane boxed sashes. There is a central panelled and glass door with a segmental hood on brackets. There is also a single-storey lean-to to the right. Northwick Lodge is located to the west of the farmhouse and is constructed of rubble, with rendered and scored ashlar to the front. It has a plain tiled roof with bargeboards to the gables, decorative eaves cornice and gable stacks. The building is L-shaped in plan, with a symmetrical two storeys and three window front. The front has a central trellis porch, and the windows are three-light casements with five panes in each light. Other 19th century features of the character area include a Grade II listed school in the centre of Northwick, Sandford's Charity School (1128953). The school is dated 1842 and is constructed of coursed rubble with stone dressings and a slate roof and comprises one and a half storeys. and the north-west front has three sets of windows, each with a six-light window with wooden mullions and transoms and glazing bars, and a gable above, with blind oval bull's eye. To the east of the school is the Grade II listed remains of the Church of St Thomas (1312454). The church was built in the 1840s, on the site of an earlier church, although now only the tower survives. The tower is built of rubble with stone dressings, in a square plan in the Neo-Romanesque style. The tower has three stages with a west door and two-light window above. There is a single light at the second stage and the third stage has paired windows with bell louvres to each side and a gabled parapet. Although no longer extant, the outline of the nave, transepts and chancel remain visible on the ground. To the eastern end of the character area is Northwick House Farmhouse and attached barn (1312459), a Grade II listed building. The farmhouse is dated 1707 with later additions. It is constructed of roughly coursed rubble and brick with stone dressings and a triple roll tile roof with gable stacks. The building is two and a half storeys with five windows to the south-east front. The second from the left has a gabled porch and the another with a stone cross-windows with entablature. There are three gables to the south-east front, the left gable has a trefoil in a square chamfered surround while the other two gables contain oval bull's eyes in stone surrounds. There is also a date stone to the central gable. The southern end of the character area consists of several detached and semi-detached houses, of 19th and 20th century date. Most are constructed of stone, some rendered, with pantile roofs. Several of the buildings at the north-western end of the character area are also 19th century and are recorded on the historic mapping. The buildings are spread along Severn Road. There is also a road of post-war semi-detached houses at the northern edge of the character area, along Aust Road. Dwellings are two storeys with pitched bays, some with porch blockwork extensions whilst others include plastic door weather coverings.



Figure 40: - Grade II Church (Tower) of St Thomas (1312454). Church rebuilt in 1840 on former marshland and condemned in 1962.

Materials	<p>The Character Area has a strong offering of heritage assets which present the vernacular material palette. Church Farmhouse is constructed of rubble, pantile roof and gable stacks. Facades are rendered and of natural colour (unpainted). The stable wing to the right is finished in double Roman tiles. Mill Farmhouse is constructed of rubble and partly rendered with a pantile roof with gable stack and ridge stack. Northwick Farmhouse is constructed of rubble and rendered, with a pantile roof and gable stacks. Northwick Lodge is constructed of rubble, with rendered and scored ashlar to the front. It has a plain tiled roof with bargeboards to the gables. Grade II listed Sandford's Charity School is constructed of coursed rubble with stone dressings and a slate roof. Church of St Thomas (tower) is built of rubble with stone dressings. Northwick House Farmhouse is constructed of roughly coursed rubble and brick with stone dressings and a triple roll tile roof with gable stacks, other bays have stone cross-windows with entablature. In the south, 19th and 20th century dwellings are constructed of stone, some rendered, with pantile roofs. North at Aust Road, dwelling facades are finished in rough cast render and uPVC rainwater goods. Contemporary additions include a timber clad over garage accommodation with concrete grey tiles and rooflight.</p>
Building modifications, extension & conversion	<p>Examples of building modification includes some house extensions and renovation of heritage building stock. Examples are in keeping, and many heritage dwellings retain timber windows. The overall balance is upheld due to the concentration of older properties with few modern additions.</p>
Sustainability and building performance, waste, recycling and utilities	<p>There is not much evidence of sustainable practices or heightened demonstrated building performance. Renewable usage is limited, despite the area's high dependency on oil for space heating.</p>



Figure 41: - Northwick Farmhouse with 1707 HWM datestone.



Figure 42: - Northwick Farmhouse with attached barn and dairy range (1312459). Incredible undulation to barn roof structure.



Figure 43: - Cycle network directional post NCN 4 & NCN 41.



Figure 44: - Manor House Farm



Figure 45: - Pound Cottage, render and snecked stone. Dark



Figure 46: - Side extension with hipped roof.



Figure 47: - Contemporary addition over garage



Figure 48: - Church Farm with pitched gabled porch enclosure.



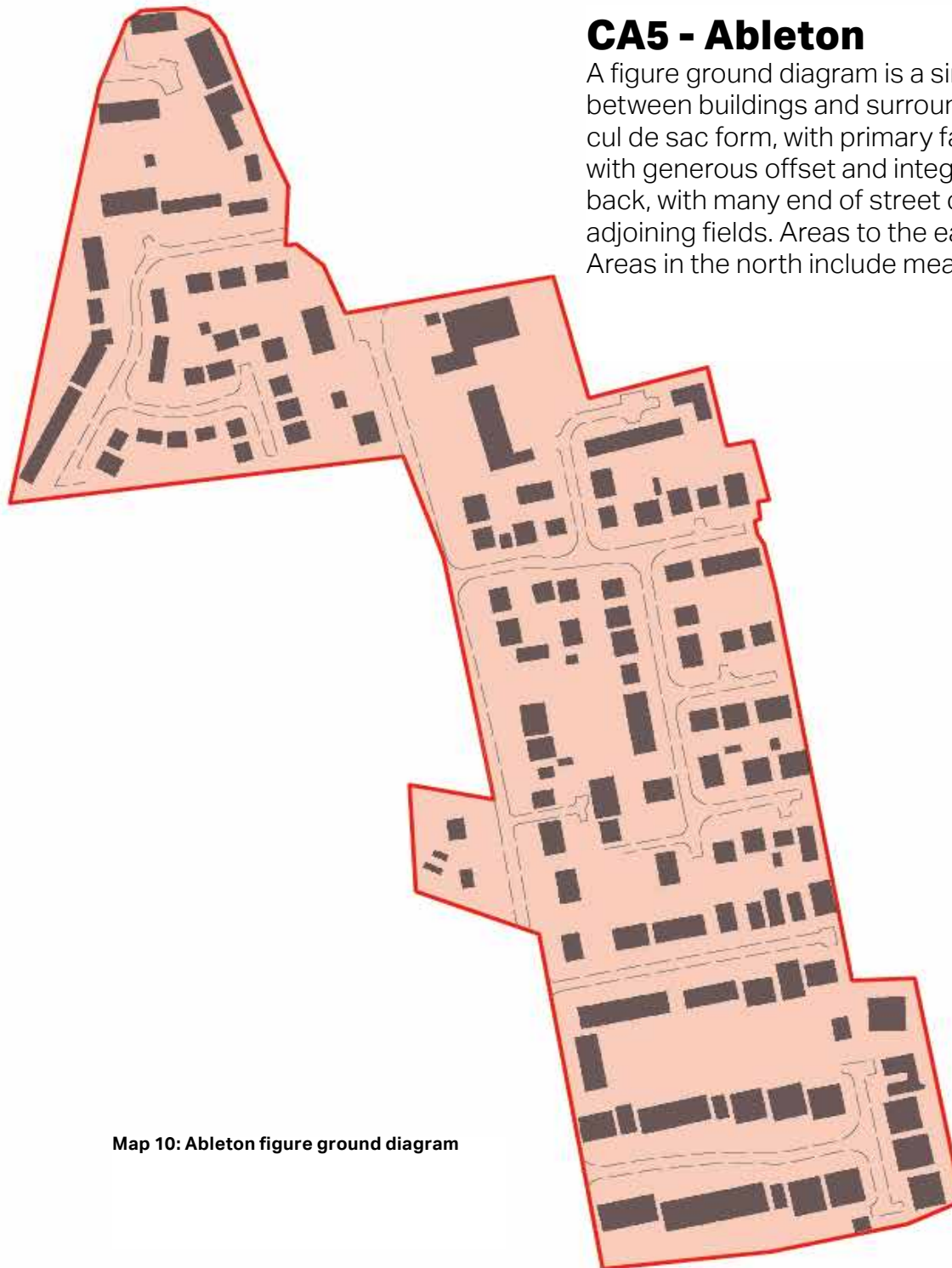
Figure 49: - Stone cottage, brick lintels, clay pantiles.



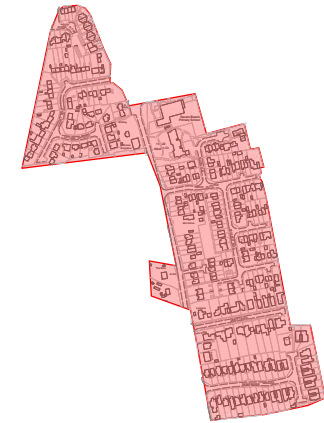
Figure 50: - Access to PRoW and typical pastoral view.

CA5 - Ableton

A figure ground diagram is a simplified plan to demonstrate the relationship between buildings and surrounding space. Street arrangement is mainly in cul de sac form, with primary façades facing the street. Building line is formal with generous offset and integrated garden frontages. Properties lie back-to-back, with many end of street cul de sacs left with scope for expansion into adjoining fields. Areas to the east of Ableton Lane are grid like in arrangement. Areas in the north include meandering streets.



Map 10: Ableton figure ground diagram



CA5 - Ableton

Pattern and layout of buildings	<p>The Character Area is framed to the north by Seven Beach, in the east by an accessible area of scrubland (Gypsies Plat), in the south by the A403 and west by pastureland and the railway line. Most areas were developed after WW2, evidenced by the spatial layout and typologies used. The arrangement densities are quite high, and there is minimal settlement function provided by the streets, except to serve residential users. Land parcels are maximised using cul-de-sac arrangements.</p>
Access, green infrastructure, active travel and open space	<p>Ableton Lane is the only primary road in the Character Area, serving as the main artery in a north/south trajectory. Secondary streets branch off, but do not loop back, routes options are therefore quite limited for vehicular traffic. There is good pavement coverage, however southern sections of Ableton Lane is single sided. Areas of small grass verges are integrated and there are some street trees, with examples such as the <i>Sorbus aucuparia</i> at the junction with Albert Road, some examples include <i>Fraxinus excelsior</i>, which are disease susceptible. One road of note is Abbot Road which is tree lined with <i>Tilia cordata</i>. The area is well served by PRoWs and the housing estates in the east benefit most from access to the network. A large area of scrubland which PRoWs cross, is a SNCI (site of nature conservation importance) and a good asset for the community, albeit not officially recognised as open space. A further small open space is located in an area west of Prospect Road. Gypsies Plat is a PRoW (ORN54), a National Cycle Network Route (NCN4) and a bridle way. It passes north to south from Church Road to the A403. Access to National Cycle Network Route 41, along the A403, is available at the southern end of Ableton Lane.</p>
Views and landmarks	<p>The topography of the Character Area and the surrounding landscape context is relatively flat, meaning views are mainly internal along streets, such as views down the A403 or at development breaks over proximate pastoral or scrubland. Most houses on Ableton Lane have views southwest (opposite) across open fields. There are no other significant views in or out of the area.</p>

Architecture and details

Ableton consists of mostly modern housing developments to the south of Severn Beach. One early building in the area is The Elms, a post-medieval red brick detached house at the western side of Ableton Lane. The dwelling is constructed in an English bond made up of alternating courses between headers and stretcher has a symmetrical front with a central doorway with a gabled porch and canted bay windows either side. The windows at first floor are casement windows, the centre window is smaller than the others. The roof has gable chimney stacks. The buildings offset from the road is greater than the majority elsewhere in the Character Area. Albert Road and Victoria Crescent include a combination of seemingly standard-form cavity construction two storey dwellings interspersed by concrete preformed panel constructed properties, some with mansard roofs. These properties have generous front and back gardens, however the economy of construction is evident with specification and window aperture sizing. At Denny Isle Drive, School Way and Prospect Road, building types include: two storey detached, semi-detached houses and semi-detached storey-and-a-half dwellings with flat roof dormer windows. Some dwellings have garages with flat roofs. The construction at first floor of the two storey dwellings is non-traditional. Constructed with a single masonry blockwork (thick) wall. Abbot Road is an example of a cul-de-sac development of single storey unit style development, with staggered fronts to incorporate bays or porch entry, some dwellings are timber frame constructed. Stride Close, the name of a local entrepreneur (Robert Stride) that built wooden bungalows in the area is designed to face front and back. Each unit is four 2-bed flats, with the ground floor front doors facing Ableton Lane and the upstairs front doors facing Stride Close. Access on both sides is via an unusual flat roofed crescent shaped porch, there are garage blocks in Stride Close. There are blocks of flats at Prospect Road and School Way, including Ableton Court, which was built as sheltered accommodation for the elderly, but is now used as temporary social housing.



Figure 51: - The Elms, brick façade, red and buff/yellow.



Figure 52: - Storey and a half with flat dormers, beside two storey dwelling with scalloped tile hanging to finish single skin blockwork blockwork wall



Figure 53: - Albert Road, generous verge with street tree. Mansard roof development with concrete panel construction.

Materials	<p>The Elms is constructed in red brick with buff brick dressings with rendered gables. The colouration of the red brick is muted with some slight variation towards orange. The roof is finished in clay double Roman tiles, with red brick stacks and coordinated yellow/ buff clay pots. Sills and lintels are cut stone (ashlar). Elsewhere the post war period is reflected through the materials specified. Brick usage is a constant although colouration ranges from reds, buff through to yellow. Render, uPVC cladding, tile hanging and reconstituted stone feature on façades. Concrete roof tile use is common throughout, some are specified flat, whilst others are in the double Romans style. Some of the original bungalows on Abbot Road have a yellow/buff clay chimney pots. Windows and doors are predominantly uPVC, specified in white or wood/dark brown.</p>
Building modifications, extension & conversion	<p>The Character Area is popular for building modification and updating, ranging from simple porch extensions to replacement roofs, new facades and dormer windows. Some of the building stock at Albert Road has been upgraded, with a new external skin added in front of the original precast concrete solid wall construction.</p>
Sustainability and building performance, waste, recycling and utilities	<p>Building stock within the Character Area mainly comprises of cavity constructed dwellings, which depending on the year of construction will include an insulated cavity. Cavity wall insulation was first introduced in the 1970s, becoming compulsory in the 1990s for all new UK buildings. Short terraces and flats built after 1990 should therefore be the best performing in energy terms across the Character Area. There are also several non-standard constructed properties, such as those at Albert Road (concrete panel) or at Denny Isle Drive/ Prospect Road/ School Way which have been constructed in a combination of cavity construction and solid wall with external insulation, façade treatments include uPVC cladding and tile hanging. In building performance terms these properties will not offer the same u-value performance and penetrating moisture resilience as standard cavity construction. The area mainly uses electricity for space heating, but despite this few houses have PV installations.</p>



Figure 54: - On-street parking opposite the Primary School.



Figure 55: - Development with standalone garages with flat roofs.



Figure 56: - View south at Ableton Lane from primary school.



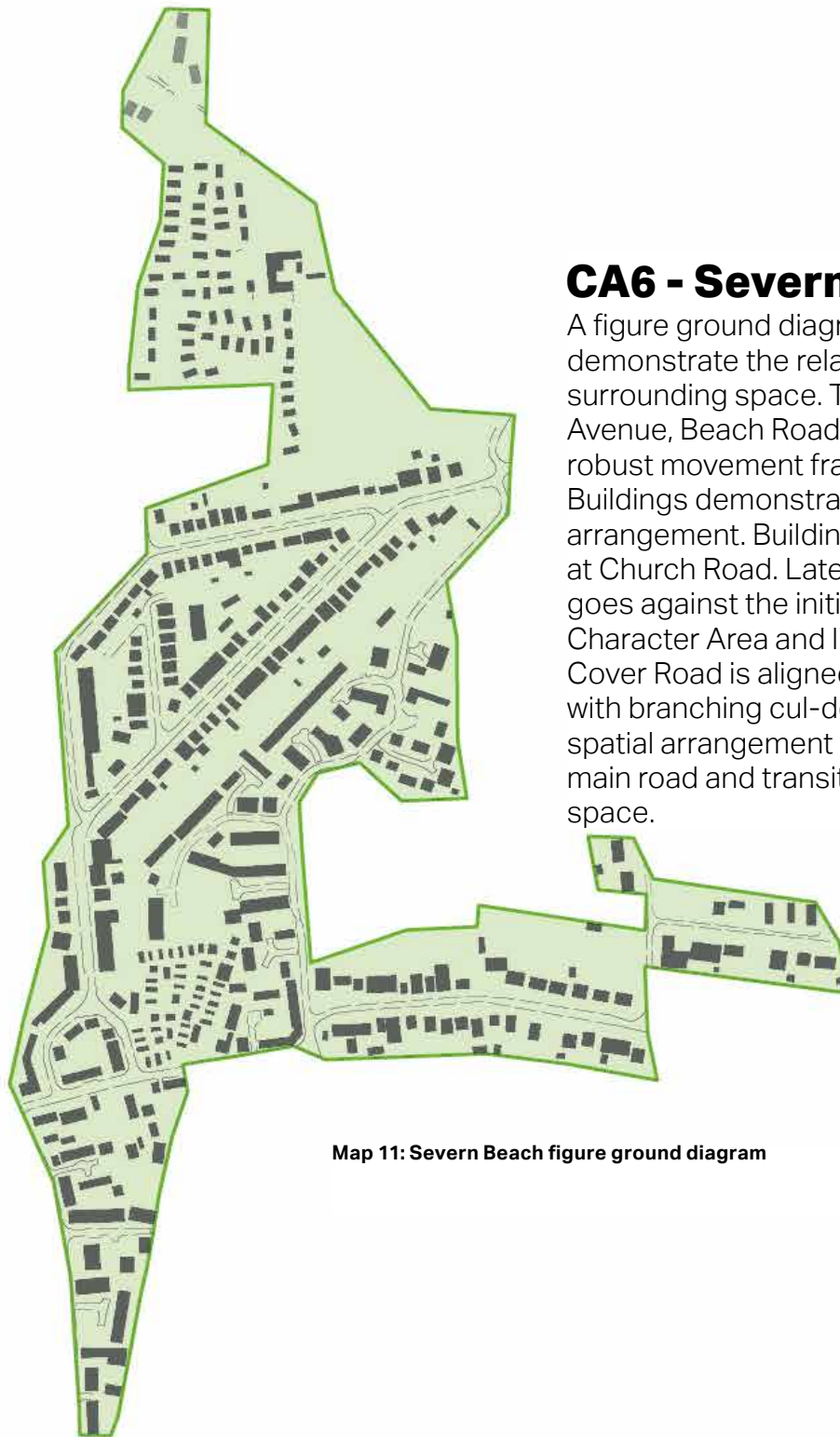
Figure 57: - Updated bungalow with lean-to porch, and loft conversion Albert Road.



Figure 58: - Concrete panel construction, Victoria Crescent.



Figure 59: - Part non-cavity constructed dwelling.



CA6 - Severn Beach

A figure ground diagram is a simplified plan to demonstrate the relationship between buildings and surrounding space. The access network at Beach Avenue, Beach Road and Osborne Road provides a robust movement framework for the Character Area. Buildings demonstrate uniformity and there is a block arrangement. Building line variation is demonstrated at Church Road. Later infill cul-de-sac development, goes against the initial spatial arrangement of the Character Area and limits settlement movement. Gorse Cover Road is aligned perpendicular to the main road, with branching cul-de-sac dense development. The spatial arrangement has a good relationship with the main road and transitioning well east toward the green space.

Map 11: Severn Beach figure ground diagram



CA6 - Severn Beach

Pattern and layout of buildings	<p>The Character Area is located to the west of the M49 and fronts onto the River Severn. Until the early 20th century the Character area of Severn Beach was dominated by agricultural fields and isolated farm. In 1912 early development was limited to Salthouse Farm, Severn Beach Farm, the GWR Avonmouth Branch, Severn Tunnel and the Sea Wall Pumping Station and associated housing (Shaft Road). By 1955 development was beginning to expand, first in areas along Church Road, a public house and other development on Station Road, Beach Avenue and Osborne Road. When the train line link to Pilning ceased operation in 1968 development (Gorse Cover Road) layer expanded (80's). Early development arrangement was quite uniform, reflecting the simple utilitarian bungalow typologies built at Beach Avenue & Osborne Road, the latter arranged perpendicular to the access. These typologies are an indication of Severn Beach's resort heyday. Larger two storey housing built at Church Road, provide early variation in typology. A property named 'Fairlawn' has a greater offset than other properties. There is a good arrangement of primary and secondary streets throughout the Character Area which provide route options and a useable network. Cul-de-sac arrangement does exist, but in some cases (Riverside Park), they are responsive to train line severance.</p>
Access, green infrastructure, active travel and open space	<p>There is a well-planned network of primary and secondary roads that provide good route options. Beach Road, which serves as the high street with commercial offerings including an excellent bakery, has a one-way section in the south. National Cycle Network Route 4 passes along Green Lane and down Little Green Lane. The Severn Way PRow provides access along the waterfront and further PRow connects Beach Avenue to Gorse Cover Road. The Character Area has a good level of green infrastructure. Severn Beach promenade can be accessed at several points' many areas in the east, and there is good non-vehicular continuous access, lawned areas and a café, the area is popular for exercise and recreation. Severn Beach Village Hall Park with Play Area and allotments north of Beach Road provide further assets for residents. Eastern boundaries at Village Hall and the Primary School include hedges with established trees which backs onto Gipsies Plat. There is a coppice of trees to the south of the Village Hall and there is a good number of trees at Ableton Court with a group TPO. Street trees exist at the northern and southern ends of Beach Road, Gorse Cover Road and new tree planting at Riverside Park, including a wetland habitat area accessible through a gated track, created by Natural England during sea wall scheme improvements. At Beach Avenue grass verges, established hedges and small trees contribute to the street scene. However, there is damage to verges caused by car parking and driveway access. Blue Infrastructure elements are provided by rhines at the rear of Beach Avenue and north-eastern areas at Gorse Cover Road. There is a lawned area with mature trees within Gorse Cover Road, and fields to the east with tree and hedge boundaries.</p>
Views and landmarks	<p>The Character Area is quite enclosed by a combination of development density, the level change (sea defence) at the promenade and the general constant topography of surrounding area. Internal street views are possible from nodal positions, such as the northern settlement gateway on Green Lane, along Beach Road, Station Road and Gorse Cover Road. Certain areas open-up and provide wider contextual views, such as at the southern junction of Beach Road and Beach Avenue, beside the café and Gorse Cover Road, with views towards Little Green Lane. At Severn Beach Village Hall, the playing field affords views of green space including a football pitch toward tree and hedge boundaries. The railway track also provides distant views towards the power station/incinerator.</p>

Views and landmarks	<p>Externally, significant views are possible from the promenade area, over the River Severn, and towards the Prince of Wales Bridge, Forest of Dean, Monmouthshire, Black Mountains, South Wales and Portishead. Shipping can be viewed passing in and out of Portbury and Avonmouth Docks, and along the Severn Estuary. Many dwellings towards the western edge of the Character Area will have some views over the Severn from upper storeys.</p>
Architecture and details	<p>Located at the north-east edge of the Character Area, Crossings House is a property built in the 1890's just after the Severn Beach to Pilning railway track was laid. The building has a clipped hip roof, pitched dormer, sash windows and cream bargeboards which match the chimney pots. The entrance is via the south-west front through a projected gabled timber porch. On the same elevation, a tall round arched window is central at first floor. A further early 20th century large two storey house at the northern end of Beach Avenue (Number 66) has a half-hipped roof and uneven fenestration. Most windows are three-light casements. Beach Avenue to the northern end of the area, and some areas of Church Road and Osborne Road, were first developed in the 1920s and comprises of bungalows units, some of timber frame construction. Many have been rebuilt, but there are some original dwellings with corrugated sheet roofs in Osborne Road. Those along Beach Avenue have gabled tile roofs, while those along Church Road are mostly designed with hipped roofs. Other areas on Church Road include two storey dwellings and a small block of flats to the West as you enter Church Road built circa 2010. At Beach Road 3 storey terraces are constructed in English Bond with brick corbels at soffit. Many of the ex-shops at street level have been unsympathetically converted into accommodation. Beach Road also contains some starter home typologies (145-161) slightly offset around the development island with a combination of parking courts, on-plot parking and lay by parking. Shaft Road row is a small development comprising a short terrace (4), 2 semi-detached and 1 detached house. In 1880 the development was limited to the smithy and two buildings associated with the pumping engine, but by 1900, the arrangement of housing recognisable today had been built.</p>



Figure 60: - Gable facing bungalow units Osborne Road.



Figure 61: - Units are popular for modification, updating the building stock that was likely thermally inefficient.



Figure 62: - Northern gateway into Severn Beach on Green Lane. Newly downgraded 20mph zone.

	Riverside Park was built in two phases in the 90's, the initial phase specified a type of brick designed to withstand the saline environment. The development contains a mix of single, double and three storey dwellings. Gorse Cover Road contains some pastiche detailing including mock Tudor panelling, roof spires and contrasting brick patterning.
Materials	Crossings House is constructed in red brick, partially rendered with a slate roof and a red brick chimney stack. Beach Avenue (Number 66) is a large two storey building constructed of brick at ground floor, tile hanging to dress the first floor, flat terracotta tile roof and red brick chimney stack. Windows are in wood/brown uPVC. At Beach Road many of the brick 3 storey terraces have been unsympathetically converted into accommodation with uPVC windows and doors, including the imposing gable ends clad with white uPVC. There is also a run of ten houses constructed in precast concrete panels with mansard roofs, although some have been upgraded. At Beach Avenue, Church Road and Osborne Road, bungalows have a range of finishes from red or buff brick, or reconstituted stone. Most roofs have concrete tiles, although examples exist with corrugated sheet metal or even asbestos roofing panels. Elsewhere later development is a mix of mainly brick construction with parts rendered. Concrete tile use is widespread, albeit style varies from double Roman to flat. Some properties on Station Road have extended height brick plinths.
Building modifications, extension & conversion	Many of the bungalow units have been upgraded, some with new walls and roof. As have examples with mansard roofs on Beach Avenue, with modifications which include the addition of an external leaf at façade. Several properties in this area have added conservatories. At the southern end of Beach Road, a recent three storey infill dwelling has been constructed. The dwelling, with greater setback from the terrace, has a monochrome façade and an unusual boundary. In more recent residential development areas, there is less modification/extension activity.



Figure 63: - Typology with resilience to flood risk, Beach Avenue.



Figure 64: - View south along promenade and green space. Incinerator in the distance.

**Sustainability
and building
performance,
waste, recycling
and utilities**

Much of the updated building stock which replaces the original bungalow units or mansard dwellings, with the correct details specified, will have improved building performance. Any surviving original units and the 3 storey brick terraces on Beach Road, with likely single skin walls, will have very poor thermal performance and internal user comfort conditions. However, generally with so much of the Character Area constructed after insulated cavity walls became standard details, the settlement is well-placed thermally. The majority of dwellings in the Character Area rely on electric space heating, however despite this there are few solar PV installations. Ashley Court is one development of note with thermodynamic panels specified. The technology is similar to solar thermal, but with better reliability in the UK climate.



Figure 65: - English brick bond, clipped hip roof.



Figure 66: - Beach Road (high street), some commercial units, some three-storey brick built development.



Figure 67: - Perpendicular placement, Gorse Cover Road.



Figure 68: - Access to parking courts at rear.



Design guidance & codes

04

4. Design guidance & codes

This section sets out the principles that will influence the design of potential new development and inform the retrofit of existing properties in the Neighbourhood Area. Where possible, local images are used to exemplify the design guidelines and codes.

4.1 Introduction

This section is divided into two parts. The first is a set of key elements to consider when assessing a design proposal. These are presented as general questions which should be addressed by developers and their design teams who should provide clarification and explanation as necessary.

The second part is the design guidance and codes, setting out the expectations of the Pilning & Severn Beach Neighbourhood Area. The elements that are more general are what we mean by design guidelines. Other elements that are more prescriptive or set out parameters are the design codes. The design principles are set out as 'Area-wide Design Guidance' and 'Character Area Specific Design Codes' to ensure they are adaptable to the unique characteristics of the Plan Area. Images have been used to reflect good precedent and demonstrate design issues for consideration.

The Area-Wide Design Guidance should apply to any residential development outside the Character Areas and the Design Codes for adjacent Character Areas should also be referred to.

This guidance advocates for character-led design which responds to, and enhances the landscape and town/villagescape character. It is important that new residential development responds to local context and enhances the "sense of place" whilst meeting the aspirations of residents. Whilst wishing to retain and enhance local characteristics, the Neighbourhood Plan Steering Group and residents are not adverse to the inclusion of some new contemporary designs in future residential developments.

This document focuses on residential development only, considering the local pattern of streets, the spatial layout of buildings, the spaces around buildings, construction traditions, materiality and the natural environment. Highlighting these to help determine the character and identity of new development, whilst recognising that new construction technologies can deliver good design also with enhanced building performance.

4.2 General design considerations

This section sets out a series of general design principles followed by questions against which the development proposals should be evaluated.

- 1 Development should demonstrate synergy with the existing settlement in terms of physical form, movement/access and land use type;
- 2 Development should relate sensitively to local heritage buildings, topography/landscape features, countryside setting and long-distance views;
- 3 Development should reinforce or enhance the established character of the settlement;
- 4 Development should integrate with existing access opportunities; public rights of way (PRoW), streets, circulation networks and understand use;
- 5 Development should explore opportunities to enhance access to public green space, to reflect settlement needs;
- 6 Development should reflect, respect and reinforce local architecture and historic distinctiveness, avoiding pastiche replication;
- 7 Redevelopment of heritage buildings including farms should aim to conserve as many vernacular features as is practicable;

8 Development should retain and incorporate important existing landscape and built-form features into the development which add richness;

9 Building performance in terms of the 'conservation of heat and fuel' over-and-above building regulations, should be a key design aspiration for new development;

10 Development should adopt contextually appropriate materials and construction details. Embodied carbon toolkits should be used to guide material specification;

11 Development should ensure all components e.g. buildings, landscapes, access and parking relate well to each other; to provide safe, connected and attractive spaces;

12 Net Zero aims should be well integrated and development should adopt low energy and energy generative technologies within the development at the start of the design process; and

13 Development should use nature-based water management solutions/ SuDS to manage on-site water and boost biodiversity habitat.



As an initial appraisal, there should be evidence within planning applications that development proposals have considered and applied the following general design principles:

4.2.1 Key points to consider when assessing planning applications

The aim is to assess all proposals by objectively answering the questions below. Not all the questions will apply to every development. The relevant ones, however, should demonstrate evidence to show how the design proposal or masterplan has responded and produced an adequate design proposal.

The following fundamental questions should be used to evaluate the quality and appropriateness of development proposals within the Pilning & Severn Beach Neighbourhood Area:

Pattern and layout of buildings

- What are the essential spatial characteristics of the existing residential development area and street pattern; are these reflected in the new proposed development?

- Are building densities appropriate for the development area?
- Is the plot to development ratio in keeping/appropriate for the location?
- How will the new design or extension integrate with the existing street arrangement?
- Does the proposal respect, incorporate and enhance local landscape features including topographic features, rhines and hydrology?
- How does the proposal relate to its setting? Have important physical and visual assets been identified and does the design respect these assets?
- If the design is within or adjacent to a heritage/designated landscape asset, have all elements which contribute to their significance been considered and respected in the new proposal? And does the new proposal preserve or enhance the setting of the asset? (Heritage assets include listed buildings and designated landscape assets include Ancient Woodland, Ramsar, SAC and SSSI etc).

Access

- Does it favour accessibility, permeability and connectivity over cul-de-sac layouts? If not, why not?
- Does the development promote active travel opportunities via building layouts, links/routes and practical equipment/spatial requirements?
- Are new points of access appropriate in terms of visibility, patterns of movement, desire lines and road speed?
- Do the new points of access and street layout pay regard to all users of the development; pedestrians, cyclists and those with disabilities?

Building heights and roofline

- Is the proposed new development building height appropriate for the location? Does it reflect the proximate scale of development and respect local area assets, existing development and views?

- Does the proposed development height compromise the amenity/privacy of adjoining properties? Does the proposal overlook any adjacent properties or gardens?
- Does the height, form and massing of new buildings respond to contextual visual sensitivities, meaning views towards development from receptor areas?
- If the proposal is an extension, is it subordinate to the existing property?

Building line and boundary treatment

- Does the proposal respect the existing building line/enclosure character and harmonise with the adjacent properties?
- Has the appropriateness of the boundary treatments been considered in the context of the site? Can boundaries/ thresholds be constructed to provide added flood resilience to properties?

Green spaces and street scape

- Has the biodiversity mitigation hierarchy and PSP3 been used to protect existing green infrastructure from development?

- Have adequate protection measures been put in place to protect existing green infrastructure during construction?
- Has site capacity for tree planting been considered and opportunities integrated, to help South Gloucestershire achieve their target of doubling tree canopy cover by 2030?
- Do proposals enhance existing green corridors and biodiversity habitat networks?
- Are there opportunities to bolster South Gloucestershire's Green Spaces Strategy, Green Infrastructure Strategy CS2 & CS24 and emerging policies?
- Have the Biodiversity Net Gain requirements been considered in accordance with the Environment Act 2021?
- In rural and edge of settlement locations does the development negatively impact visual character or interrupt existing tranquillity and has this been fully considered with sufficient mitigation included?
- Have nature-based water management solutions/ SuDS (PSP20) been integrated

to manage on-site water and the area's flood risk?

- Will any communal amenity space be created? If so, has usage been considered? and are measures incorporated to successfully fund landscape maintenance work?
- Have aspects of active and passive security been fully considered and integrated within development?
- Is active travel promoted at street level, and has the masterplan been designed to connect to existing movement networks?

Views and landmarks

- What are the existing key views and visual landmarks in the area and have these been retained, incorporated or enhanced by the development proposal?
- Does the development fall within any areas of key settlement views? How are these respected in the design?
- Are new views of the existing settlement and surrounding area incorporated into the proposal?

Architectural details and materials

- Has the local geology and architectural character been reflected in contemporary or traditional design proposals?
- Do the proposed materials harmonise with the local vernacular and geology? Are the construction details and materials of sufficient high quality?
- Can local materials be specified to support local industry?
- Has material specification considered user maintenance? Have appropriate materials been considered which provide longevity and robustness? Have developments within saline environments considered appropriate material finishes and fixtures?
- Has flood resilience been designed into development?
- Does new development demonstrate strong design rationale, quality material specification and good detailing appropriate for the local climatic conditions?
- Is building performance a priority,

relating to sustainability, running costs and user enjoyment?

- Has a fabric first approach to energy efficiency been integrated as a primary design driver? Are there opportunities to improve the thermal performance of the building fabric and future proof development?
- Have window, door, eave, verge and roof details been refined and considered in response to microclimates?

Parking and utilities

- Has a combination of well-integrated on-street parking and on-plot parking been integrated? Do proposals align with Policy CS8 and are they appropriate for the NP Area?
- For appropriate housing typologies, are there opportunities to accommodate mobility vehicle storage areas?
- Does new development include fast internet speeds and space to work from home?
- Has adequate provision been made for bin storage, including areas for waste separation, holding and recycling?
- Is the location of bin storage facilities appropriate in relation to the travel distance from the collection vehicle?

- Does the installation of utilities include appropriate access for maintenance/servicing?
- Is the use of renewable energy and energy saving/efficient technologies encouraged and maximised? Does the strategy qualify for the Domestic Renewable Heat Incentive (RHI)?
- Are all utilities and technologies well integrated with the building design?
- Have opportunities been explored to link with nearby industrial/commercial operations in an co-locating energy strategy (Policy CS4)?
- Does the lighting strategy reflect the strategy of the settlement for both private and public lighting applications?

4.3 Design codes

Design Code 01

Pattern and layout of buildings

Area-wide Design Principles

1. Development masterplans should be considered strategically at settlement level, in combination with South Gloucestershire strategic policy. Contextual analysis should explore opportunities to integrate elements which improve settlement functions, drive policy ambitions and increase quality for residents;
2. Development should adopt the predominant enclosure and density characteristics demonstrated in the surrounding context or evolve the design to create spatial or placemaking improvements;
3. Disparate residential developments should be resisted, instead developed to connect to existing development and create cohesive sustainable communities;
4. New residential development should be designed to be permeable, with new development roads providing strategic function to settlement users. The use of cul-de-sacs which do not loop should be resisted;



Figure 69: - Northern gateway to Severn Beach. Meandering street, generous offsets and garden frontages. Inclusion of street verges.

5. Development should provide space to design-in non-vehicular route options with provision for pavements and access tracks which connect to existing PRowWs or other existing developments;
6. Development building layouts should integrate contextual views where possible, and there should be adequate provision to create habitat corridors and nature-based engineering solutions for flood resilience and water management;
7. Development edges should be highly considered. Development should engage/mesh with existing development edges and building elevations should project an attractive and positive frontage;
8. Development should gradually transition to the surrounding landscape context. A good example of this is the eastern edge of Gorse Cover Road; and
9. New residential development layouts should respond to site specific microclimates to harness opportunities to improve thermal efficiency, reduce energy consumption and increase the environmental comfort for building users, both internally and externally. The openness of the River Severn provides opportunities and threats related to harnessing passive gains and exposure.



Figure 70: - Space, views, green infrastructure and water management combine to create richness and better quality places. Severn Beach.

Pattern and layout of buildings

Character Area Specific Design Codes

CA1 - Pilning

1. The current settlement structure deviates from settlement origins with an overuse of non-connecting settlement roads. Possible future developments should therefore favour settlement connectivity, by providing streets that offer connections and practicality for all users.



Figure 71: - Lack of inter-development connectivity.

Wainbridge Crescent and Cranmoor Green, could have been improved by the provision of a connecting green space.

CA2 - Redwick

2. The Character Area has three distinct approaches to building line and arrangement character; including formal, informal and nucleated. Possible future developments should therefore adopt one or a combination of these approaches, to respond to contextual features such as landscape and roads, but should ensure development edges are well articulated and present positively.



Figure 72: - Unresponsive development arrangement.

Development must respond positively to contextual elements.

CA3 - New Passage

3. Area precedent which should be integrated within possible future developments includes: buildings orientated towards north east for southern areas of New Passage Road and the perpendicular arrangement of Bland's Row.



Figure 73: - Spatial arrangement of Bland's Row.

Terrace arrangement perpendicular to main access.



The diagram illustrates methods of achieving a gradual transition. In this example a more informal building line, with greater variation of offset is used to allow more space for front gardens and green infrastructure (GI) components to help the transition. A road with large pavement including grass verge to create opportunities for SuDS and separation between bicycle lane and vehicular traffic, and structured tree planting helps with visual screening.

Character Area Specific Design Codes

CA4 - Northwick

4. Possible future developments should respect the settlement's heritage farm core and not cause a dilution of character. For areas in the south of the Character Area, it would be in keeping for pocket development to adopt the same farming cluster style arrangements demonstrated in the farm core.



Figure 74: - Development clusters increase depth.

Example of farm character.

CA5 - Ableton

5. There is a grid uniformity to the area which should be brought forward into possible future development areas. With improvements made to the access network and a reduction on cul-de-sac arrangement.



Figure 75: - Cul de sac overuse.

New residential development should provide a functional settlement use beyond simply housing.

CA6 - Severn Beach

6. There is a linearity and grid characteristic to early residential development areas and roads, with some development areas formed in blocks. Possible future developments should follow these purposeful arrangements, through the creation of block typologies and explore opportunities for integrated mews blocks.



Figure 76: - Example of block articulation, junction of Osborne Road and Beach Avenue. Beach Road can be seen behind.

Block typologies would be in keeping in this Character Area.



Figure 77: - Pilning central area.

Design Code 02

Access, green infrastructure, active travel and open space

Area-wide Design Principles

1. New residential development streets should be tree lined and boundaries bolstered to meet South Gloucestershire Councils tree canopy targets;
2. Developments should be designed around the retention of existing green infrastructure elements such as trees and field boundaries by applying the biodiversity mitigation hierarchy;
3. A development green infrastructure plan should be designed around biodiversity habitat connectivity, seeking out opportunities to bolster and bridge gaps and improve the overall habitat network;
4. Development should be designed around Biodiversity Net Gain requirements;
5. Contextually appropriate species should be used, with schemes designed to thrive in the micro-climates, soils potential saline environment specific to the Pilning and Severn Beach Neighbourhood Area;
6. New development planting should be cognisant of, and resilient to climate change. A combination of native and well-considered non-native species should be used;



Figure 78: - BS 42021:2022 sets out requirement for Integral nest boxes.



Figure 79: - Impressive green and blue infrastructure urban assets, Govier Way.

7. Opportunities brought via new development such as CIL or other initiatives should explore opportunities for further green or blue infrastructure initiatives;
8. Nature based solutions, including SuDS water management should be integrated to improve the areas flood resilience;
9. Opportunities to connect residential development to existing PRoW and cycle networks should be explored to help boost settlement active travel options;
10. Where practical, traffic calming measures should integrate SuDS or green infrastructure;
11. Surface materials should be considered with a preference for permeable combined with robust details to provide longevity;
12. Turf with plastic netting or artificial turf should not be specified;
13. New development should incorporate bolt-on features to assist biodiversity including bat bricks, bird boxes (BS 42021:2022 Integral nest boxes) and hedgehog gravel boards; and
14. Green Infrastructure, open space, play spaces, community allotments or orchards should be explored and integrated to meet community needs. There should be opportunities to connect with nature at different scales everyday.



Figure 80: Good example of a functional verge, both supporting biodiversity and water management.

Access, green infrastructure, active travel and open space

Character Area Specific Design Codes

CA1 - Pilning

1. New developments should integrate improved non-vehicular access networks and green corridors. Improved signage should support the existing PRow networks.



Figure 81: - Lack of signage to support PRow network.

Emphasis is on development to fill the shortfall in non-vehicular connectivity due to the current lack of pedestrian and cycle access.

CA2 - Redwick

2. Redwick has an almost continuous green boundary at the rear of development throughout the Character Area. New development should integrate tree planting to complete the network and provide full habitat connectivity.



Figure 82: - *Malus* tree planting, hedge and small paddock, street level green infrastructure.

Development should be used to provide further enhancements to the Character Area's green infrastructure.

CA3 - New Passage

3. Access to the River Severn is very popular and prone to car parking congestion close to Caroline Cottage. To improve settlement function, enhanced road management with markings and the provision parking facilities should be brought forward via development.



Figure 83: - Area susceptible to on-street parking issues.

Placemaking and functionality opportunity beside River Severn.

Character Area Specific Design Codes

CA4 - Northwick

4. New residential development should be designed to link into the existing non-vehicular networks and integrate elements of green and blue infrastructure.



Figure 84: - Local PRow network.

Access to non-vehicular route options increase active opportunities.

CA5 - Ableton

5. Retrospective opportunities to link existing development to Gipsies Plat should be explored, brought forward by new development funding, to provide non-vehicular connections which help to daisy-chain development areas.



Figure 85: - Lack of connections, despite opportunity.

Opportunities missed for more connected communities.

CA6 - Severn Beach

6. SuDS design rationale should be applied to all new developments with adequate space provided for green and blue water management installations.



Figure 86: - Lack of green infrastructure leading to increased water run off.

Opportunity for permeable parking laybys in area with heightened flood risk.

Green area at street terminal acts as waymarking/placemaking tool.

Meaningful garden frontages help create a local habitat network.

Linear elements are important for biodiversity.

Multifunctional green infrastructure, has many benefits.

Well integrated functional green infrastructure can be used to deliver measurable development benefits (Biodiversity Net Gain) and multifunctional community benefits such as: active travel, reduced surface water and associated flood risk, air pollution filtration, noise abatement, supports biodiversity and a reduction in urban heat island effects. Green infrastructure should be meaningful and designed into development at different scales to create everyday interactions between nature and residents.



Figure 87: 9 - Pleached tree planting, *Tilia europaea*.

Design Code 03

Views and landmarks

Area-wide Design Principles

1. Robust visibility analysis should be demonstrated to understand key visual receptors;
2. Demonstrable design awareness should inform responsive development layouts, with material specification used to break-up development massing and appropriate mitigation such as trees and landscaping used to integrate new development within its surroundings;
3. Resident privacy should be designed into new development and care should be taken to maintain the privacy of existing developments. Built form which responds to street articulation is an opportunity to build-in privacy; and
4. Passive surveillance can help increase security for development and views towards community assets can increase the feeling of personal ownership. Designers should integrate opportunities for passive surveillance particularly towards streets, open/play space and pedestrian/cycle access.

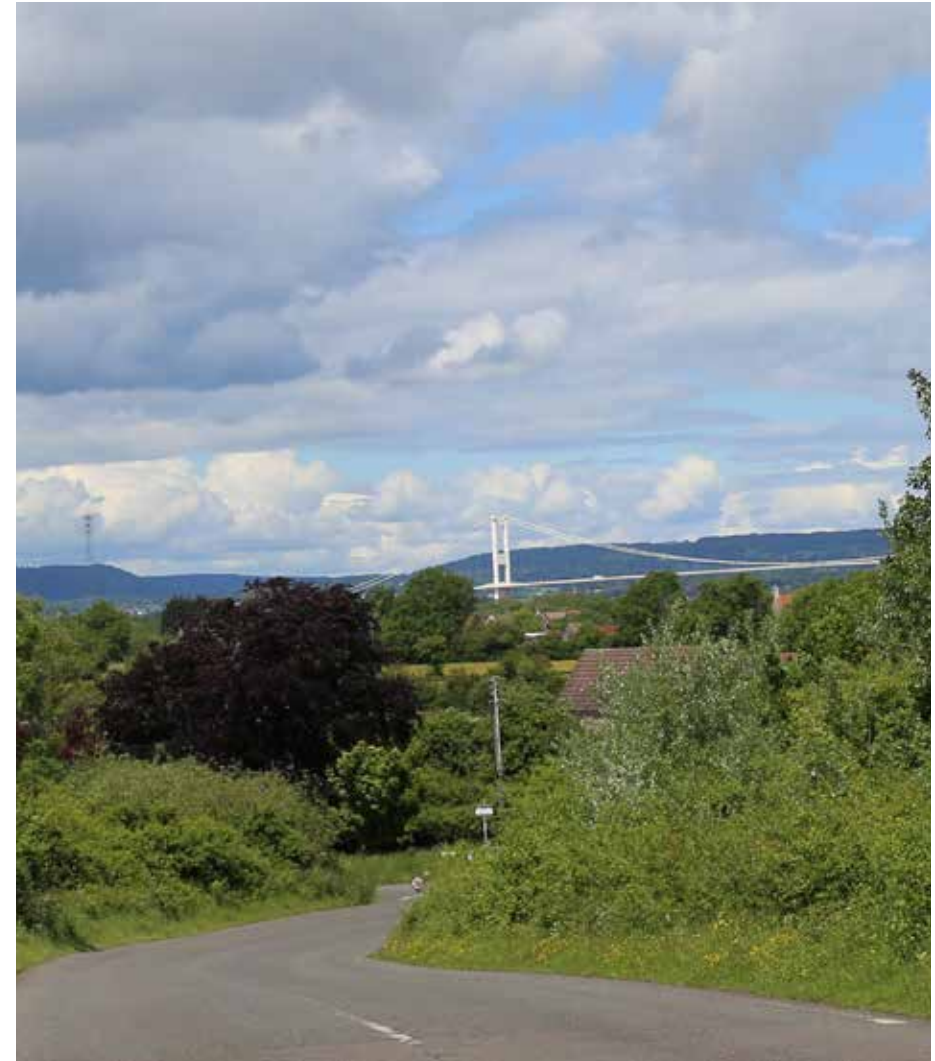


Figure 88: - View towards M48 Bridge, Northwick.

Views and landmarks

Character Area Specific Design Codes

CA1 - Pilning

1. Inner settlement views are the main type of views in Pilning. Therefore the setting and quality of existing assets should be the main focus for protection and enhancement brought forward by development.



Figure 89: -Opportunities for enhancements to existing landmarks and placemaking.

There are opportunities for asset improvement and the integration of views in development toward assets.

CA2 - Redwick

2. Key settlement assets such as the areas surviving post-medieval landmark buildings should be identified and their 'setting' carefully considered and protected from inappropriate adaptations and erosion of character brought by new development.



Figure 90: - Heritage asset, uPVC windows and signage.

Local heritage landmarks should ensure the quality of material and adaption is appropriate and in keeping with the heritage asset. Low quality signage and uPVC canopy's detract in this example.

CA3 - New Passage

3. Local landmark elements part of the New Passage heritage trail should be given due respect in the planning of new residential development.



Figure 91: - Local celebrated heritage.

The identification of local assets can help to protect and integrate them as placemaking components.

Character Area Specific Design Codes

CA4 - Northwick

4. The setting of the historic farm core and church tower should be respected and not degraded by new development. Viewpoints from more open areas of the Character Area should be considered and development sensitively integrated.



Figure 92: -Example of expansive view, Northwick Road.

New residential development must consider visual sensitivity and be sited and mitigated appropriately.

CA5 - Ableton

5. Edge character must be considered and be designed to project positively towards receptors.



Figure 93: -Edge character, view from A403.

A lack of edge character quality can erode settlement appeal, here a well-chosen green infrastructure element could improve settlement character, reduce road noise, capture air contaminants and create

CA6 - Severn Beach

6. New development where appropriate in western extents could adopt three storey typologies to increase flood resilience and provide River Severn views.



Figure 94: -Three storey apartments, Station Road.

Appropriately sited developments could increase building height to provide multiple benefits for residents including views of the River Severn.



Figure 95: - Lowland pasture, Pilning Street.

Design Code 04

Architecture and details

Area-wide Design Principles

1. Net Zero collective ambitions and development go hand in hand. Building efficiency regarding the conservation of heat and fuel and sustainable practices regarding material sourcing and waste should be key development considerations;
2. Details, junctions, and materials should be scrutinised with development and encouraged to go beyond u-value building regulation targets - to future proof development;
3. New development should respond to the scale and form of existing buildings, with internal space standards generous and appropriate for modern use;
4. Architectural design should demonstrate design lineage derived from existing high-quality vernacular and contemporary examples;
5. Architectural variety should be integrated within new residential development areas, though a designed response to existing environmental or built constraints;
6. Internal fit-out should be robust with consideration for the passage of sound between internal rooms and neighbouring properties;
7. Frontage variation (set back) and building height should be used and designed as an urban design control tool to provide enclosure, focus or expansion where appropriate;
8. Consideration should be given to the adoption of flood resilient typologies and arrangements that locate high priority living areas above ground floor;
9. Flood resilience must be integrated into new development through a combination of passive and active control measures. For buildings, boundary walls, gates, ground floor levels/thresholds, plinths, lower storey facades must be cognisant and respond to the elevated flood risk. Where ground raising is proposed to address flood risk, level for level compensation should be included;



Figure 96: - Clipped hip, gabled porch and lower storey pitched bays.

10. Foundation and subfloor details must be aligned to the water resilient strategy adopted by the designer, taking an approach either as a water exclusion strategy or water entry strategy;
11. Traditional pitched roofed houses should continue to be the predominant typology. Flat roofs for buildings, extensions, garages, dormer windows and porches should be avoided, however, flat roofs with ecological green/ brown roofs are acceptable;
12. Chimney inclusion should be cognisant of Net Zero and sustainability targets. If chimneys are included, chimney height should be proportionate to the dwelling. Chimney position and height should consider neighbouring property window locations and prevailing winds;
13. Decorative brick bonds such as English, Flemish or English Garden Wall bond should be integrated to create visual interest and to accord with settlement character;
14. Window placement should be offset from the eave by a minimum of 150mm. Consideration should be given to eave detail, to ensure spacing if boxed eave and soffit is specified; and
15. Generous eave and verge overhangs should be sufficiently large enough to throw rainwater clear of the façade to provide improved weather resilience and minimise external surface staining, especially in exposed estuarine locations.



Figure 97: - Open sided (pitched) porch with double Roman tiles.

Architecture and details

Character Area Specific Design Codes

CA1 - Pilning

1. Roof style variation should be incorporated into new development, as is evidenced across the settlement.



Figure 98: -Good example of roof variation.

Developments which do not reflect this attribute of settlement character should be resisted.

CA2 - Redwick

2. Pitched gabled porches are a common feature across the Character Area, that provide useful space, weather protection, and add richness to the street scene. New development should reflect this characteristic by designing-in gabled porches either on the front or side elevation. Small gabled projections should be included as a design feature even in areas limited in space.



Figure 99: -Example demonstrates feature where space is restricted.

Porches should be built floor to roof to provide full weather protection. Front door canopy's should be resisted.

CA3 - New Passage

3. A gabled/verge parapet and buildings arranged with a facing gable are distinctive roofing details which exists within New Passage, and therefore new development should integrate these details accordingly.



Figure 100: -Facing gable and verge parapet detail.

Example of roofing details which should be integrated within new development.

Character Area Specific Design Codes

CA4 - Northwick

4. Twin gable chimneys is a design feature which should continue to be integrated into new development. Alongside standard functions, designers should be encouraged to explore alternative uses for chimneys such as for venting utilities.



Figure 101: -Chimney bookend.

Alternative use will provide longevity for the architectural design feature as we move towards Net Zero.

CA5 - Ableton

5. For improved resilience, all roofs of the primary and subordinate parts of the dwelling such as dormer or garage must be pitched, unless a flat ecological green/ brown roof is specified;



Figure 102: -Flat roof dormers, reduced performance and space.

Pitched or ecological green/ brown roofs is a simple unifying detail to improve Character Area quality.

CA6 - Severn Beach

6. As a flood control method, new development should consider habitable rooms at first floor level. Otherwise, ground floor levels should be designed above modelled flood levels and dwellings should be designed with solid garden walls where practicable, providing options for control gates. Specification and detailing should include flood control threshold gates, non-return valves to drains, electrical and mechanical components set above predicted EA flood levels, and consideration should be given to interior finishes such as cement based wall treatments, flood resistant kitchen units and non-absorbent flooring.



Figure 103: -Flood resilient typology, Beach Avenue.

Passive control methods provide heightened resilience. Garden wall combined with control gate provides good resilience options.

“Sense of place”... what does it all mean?

To create successful meaningful places, which are representative of the people that live there, settlements must evoke a “sense of place”. These three words are regularly used by designers to describe the unique and special qualities of settlement character, but this is not easy to recreate. Settlement character evolves slowly over centuries, as a coalescence of everyday practices, shaped by people and place. Settlement buildings are one aspect that documents this history, with architectural building styles and materials illustrative of the unique relationship and response people have to their environment.

It is paramount therefore, that new development must rise to the challenges of the future, whilst carrying the legacy of settlement character by designing new places that are richly identifiable, innovative and capture the “sense of place” of the settlement.

This does not mean that new development should copy or recreate buildings from the past, but the design must demonstrate a firm understanding of the principal aspects of settlement character and express this through the architectural style and material specification of new buildings, this is what is referred to as design lineage.

The following images taken from outside the parish, have been included to give a clear example of innovation in new development that incorporates the settlement character of the existing settlement, thus demonstrating the expectations of the Pilning & Severn Beach Neighbourhood Group.



Figure 106: - Example of clear design lineage and innovation (new development).



Figure 107: - Example of original dwellings.



Figure 108: - Contrasting brick, window offset from lintel, decorative bond (English bond) and double Roman tiles.

Details and materials

The following images illustrate some good examples of Neighbourhood Plan Area building details and material choices that both evoke the character of the area, and set an achievable precedent for developer adoption:

Red pantile roof;

Combination textured render painted (muted colour) and stone-faced building;

Chimneys with pots;

Window distance from eave; and

Frontage with estate railings.



Offset from eave

Pantile & chimney pot

Contrasting complimentary materials



Details:

Roof

Contrast

Boundary

Detail

Rough cast render;
Clay (red) pantile roof covering with façade
paint that is complimentary;
Dual gable chimneys with clay pots;
Stone at sill, plinth and planter;
Fenestration rhythm and timber sash;
Parapet at verge and front;
Window distance from eave; and
Combination masonry and timber open

Gable stack with
clay pots and
pantiles



Window materials
& rhythm

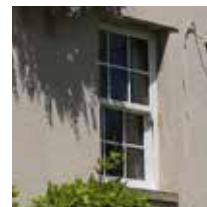
Stone accents



Details:



Roof



Materials



Plinth



Offset

- Double Roman Tiles
- Pitched dual porch access;
- Dwarf stone wall with chunky timber piers;
- Window spacing from eave and surround;
- Brick chimneys with pots; and
- Muted painted façade compliments the red clay tiles.



Material & character

Double Roman red clay tiles

Material use and component sizing



Details:

Roof

Surround

Detail

Material

- Pitched open porch;
- Pitched bay windows;
- Arched window lintel;
- Window offset from eave;
- Gable chimneys; and
- Flat red tiles

Pitched porch
and bay
windows

Brick piers and
metal gate

Gable
chimneys and
clay pots



- Red double Roman tiles with contrasting ridge tile, rendered chimney (both gables), brick detail and clay pots;
- Generous window apertures and timber sash windows;
- Open porch detail, dwarf wall, timber framing and scalloped lead roof and scalloped lead roof; and
- High quality metal boundary and gate.

The dwelling's general proportions combine with well sized architectural details, to help enable the use of multiple decorative features at fascia, gutter, quoin, plinth and porch to be presented in an aesthetic which is not congested or busy.



Muted facade, with contrasting window, door and roofing elements.

Well detailed open sided porch



Design Code 05

Materials

Area-wide Design Principles

1. Building user should be considered to ensure maintenance/longevity of materials accord with the inhabitant group;
2. The impact of material specification on street/settlement character should be considered. Material evolution should not be constrained, however there must be evidence of material lineage;
3. On exposed building elevations subject to prevailing winds, saline environments and driving rain, materials should be considered carefully, and appropriate resilient façade treatments specified;
4. Brick is a practical construction material common across the Neighbourhood Area. Early buildings constructed specify a red through to orange brick with some colour variation across the façade. Buff bricks are also used (sometimes) pick out a particular detail. Later development has introduced a range of different coloured bricks and has diluted settlement character. Careful attention should therefore be paid to matching the tonal attributes of the principal brick used early settlement buildings for new;



Figure 109: - Red clay tiles with complimentary facade materials.

5. Flood resilience and material specification are intrinsically linked. Foundation and ground flood specification should consider water penetration and drying ability. Concrete blocks used in foundations or at ground level should be avoided or must be sealed with an impermeable material to prevent water ingress into the cavity. Dense materials such as cast concrete or engineering bricks have improved water resilience;
6. Net Zero should be a key specification consideration. Green guides should be used to source sustainable products, with a preference for locally made low embodied carbon materials;
7. Stone construction is seen across the Neighbourhood Area in building elevations and boundary walls. Stone should continue to be specified which reflects the local geological character, reconstituted stone or stone slips should be avoided;
8. Clay tiles, either double Roman, pantiles or flat, accord with local character. The colour of building construction material or the façade finish must accord with the red colour of clay tiles; and
9. Innovation which explores the integration of green/brown roofs which align with a general SuDS strategy should also be encouraged.



Figure 110: - Vernacular material palette which includes integrated extension.

Materials

Character Area Specific Design Codes

CA1 - Pilning

1. Combinations of brick, render or stone should form the primary construction material, with roof finished in clay tile. Quoin details if specified should be articulated as part of the buildings structure, not in the form decorative slips.



Figure 111: -Proportions and material usage.

Material quality should be a key design pursuit used to increase settlement identity.

CA2 - Redwick

2. Rendered façades should not be the only façade material/ finish, instead if render is specified, it should be used in combination with brick or stone.



Figure 112: -A brick or stone porch or exposed gable would help increase dwelling character.

The oversimplification of façades can reduce settlement character. Render is best combined with either brick or stone.

CA3 - New Passage

3. Stone or render should be the primary façade construction/treatment within New Passage. Timber cladding for outbuildings or garages could be considered. Brick is not widely used here, with only a few post war exceptions.



Figure 113: -Material palette combinations.

The material combinations here demonstrate a range of 'in-character' finishes.

Character Area Specific Design Codes

CA4 - Northwick

4. Street facing stone boundaries should be specified for all new development to accord with settlement character.



Figure 114: -Stone wall boundary combined vegetation, stone piers and timber gate.

Stone boundaries are almost universally adopted in Northwick.

CA5 - Ableton

5. uPVC cladding or cement tile hanging should be resisted, with a preference for the specification of natural cladding materials such as timber or clay tile.



Figure 115: -Clay tile hanging could help improve settlement character.

Opportunity to increase settlement character through the specification of clay tiles.

CA6 - Severn Beach

6. To create improved settlement unity, building specification should include a minimum of either red clay tiles or red brick to part of the facade to match the vernacular tones.

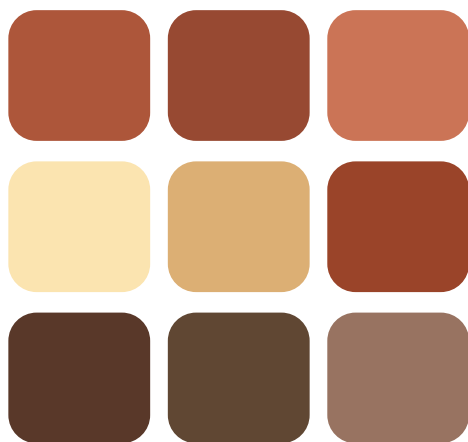


Figure 116: -Not an issue with quality, but lack of settlement identity.

Material choices have become generic and lack the connection to place.

Brick is specified across the Neighbourhood Area in a range of colours, illustrative of the material evolution of the settlement. Early buildings specify a red through to orange brick with some colour variation across the façade. Buff bricks are also used (sometimes) to pick out a particular detail. Later developments introduced a different range of colours which has led to a dilution of settlement character and, in some cases, degraded character. One important consideration is the colour synergy between brick façade and roof material which should be balanced and well considered.

Examples have been included here to summarise the vernacular tonal attributes and façade variation which new development should accord with when specifying brick. It is recognised that colour alternatives to these can be specified for brick plinths, lintels or sills.



The Neighbourhood Area has a strong design characteristic represented in the specification of red clay tiles. Material evolution has introduced the use of concrete tile and standing seam which is at variance to settlement character.

The principal roofing material to accord with settlement character should be clay tiles, specified as double Roman, pantile or flat. Variation is acceptable when there is good reason, or part of contrasting or minor architectural addition, such as dormer, garage or porch.



Double Roman.



Pantile.





Figure 117: - Diminishing stone courses on gable wall.

Design Code 06

Building modifications, extension & conversion

Area-wide Design Principles

1. Existing precedent for EWI (External Wall Insulation) application to mansard or bungalow units should be encouraged across the Neighbourhood Area;
2. Extensions should always be subordinate and the original building should remain the dominant element regardless of the number of extensions;
3. Mechanisms to increase flood resilience should be integrated within all extensions;
4. The architectural style of an extension should accord with the host building, using the same or complimentary design language;
5. Modifications to existing buildings should preserve and if possible, enhance the existing building's architectural style;
6. Extensions and plot infill should not cause unacceptable detriment to the privacy of neighbouring dwellings, the design should be cognisant of window position and rights to light;





Figure 118: - uPVC overuse, erosion of design quality.



7. The thermal upgrading of heritage assets is an important stage in renovation. Internal wall insulation (IWI) details should be cognisant of interstitial condensation risk and continuous vapour control specified;
8. Modern design with contrasting high-quality materials is often the best approach for extensions to listed or heritage buildings of significance, with clear definition between old and new. For more information see: <https://www.spab.org.uk/advice/alterations-and-extensions-listed-buildings>; and
9. External working from home office spaces should be well designed, provide enough natural light, be thermally efficient and secure.
10. Many extensions outside of designated areas are allowed without planning permission under permitted development rights, for more information visit <https://www.planningportal.co.uk/permission/common-projects/extensions/planning-permission>




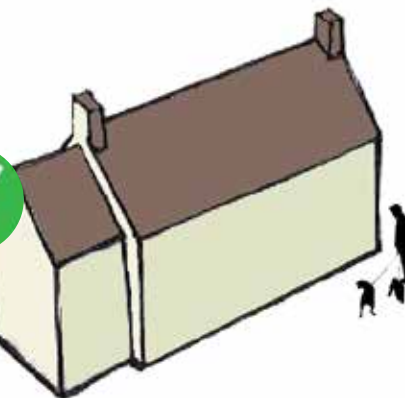
Figure 119: - Unusual building arrangement.

Good example for side extensions, respecting existing building scale, massing and building line.








Flat roofs are best avoided and extensions should always consider shadowing.

The extension has an appropriate scale and massing in relation to the existing building. Ancillary structures should be subordinate to the main dwelling.

Design treatment in case of loft conversion:

Loft conversion incorporating skylights.







Loft conversion incorporating gabled dormers.

Minimum 0.5m recess from edge of gable to dormer.

Maximum height of dormer not to exceed existing ridge.

Avoid side windows that could harm neighbours privacy.

No extension to project forward of original roof footprint.

Original roofline of an existing building.

Loft conversion incorporating gabled dormers.

Dwelling design does not consider existing window rhythm nor frequency.

Code: Extensions

Extensions provide building flexibility for residents to meet their growing family or other spatial requirements, without the inconvenience of moving. An extension can transform a property's appearance and increase its functionality (WFH). The design of extensions should therefore be used as an opportunity to enhance dwellings, and therefore extension type, position and materials should be considered robustly.

Building modifications, extension & conversion examples:

Building updates can provide home flexibility for residents to meet growing family or other spatial /adaptation requirements, without the inconvenience of moving.

Here are examples from the Neighbourhood Area that have been carried out successfully:



Figure 124: - Large two storey extension. Good overhang continuation over garage.



Figure 125: - Positive replacement/rebuilt bungalow unit examples.



Figure 126: - Successful contemporary build, but could have been improved through the specification of a clay tiles roof.



Figure 127: - Double bay with integrated porch.

Design Code 07

Sustainability and building performance, Waste, recycling and utilities

Area-wide Design Principles

1. The partial lack of mains gas in Pilning & Severn Beach Neighbourhood Plan Area, means there is an interesting space heating dynamic and opportunity for renewable technology/low carbon options. Development should therefore consider alternatives aligned to the UK's Net Zero ambition (No new gas boilers will be sold after 2035);
2. Energy partnerships with industrial zone partners should be explored to identify innovative solutions to space heating or electricity generation;
3. To meet the rise in electrical household demand, new development should consider on-plot renewable strategies including installed solar PV, or explore development-wide decentralised strategies;
4. Passive design principles should be applied to all new residential development first, with opportunities sought to harness gains and reduce losses by passive or natural means;

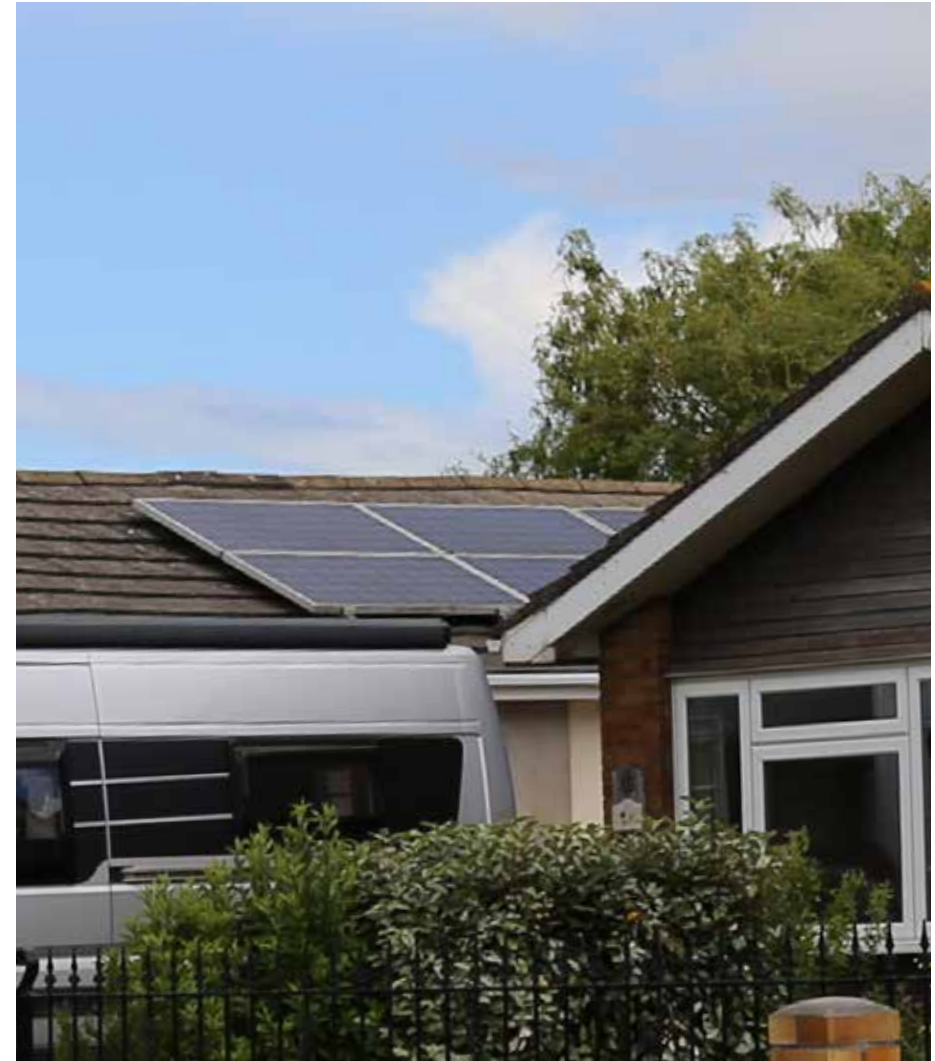


Figure 128: -Despite the lack of mains gas, uptake for PV is fairly limited.

5. All services/utilities specified should ensure cables and ducting etc are well integrated. PV, solar thermal, thermodynamic or other building mounted services should be located discretely. For heritage or listed buildings, installation should respect key elevations of the historic asset and not be street facing;
6. Meter boxes can clutter building façades, therefore maintenance and meter reading should be considered, and utility metres boxes located discretely and not on primary façades;
7. Simple rainwater harvesting facilities such as water butts should be integrated within scheme design to reduce water consumption;
8. Opportunities for grey water harvesting and reuse for non-potable requirements should be explored;
9. Sensitive lighting design should accord with the light strategy of the Neighbourhood Area. Development lighting design should consider ambient lighting levels and identify sensitive receptors to artificial lighting. Incorrect lighting specification can impact neighbouring properties and be harmful to wildlife including at designated sites; and
10. Electric vehicle charging and mobility vehicle charging should be considered and integrated where there are developmental requirements.



Figure 129: -Sandfords Charity School, Grade II Listed (35339), sedum roof covering.

Sustainability and building performance, Waste, recycling and utilities examples:

Here are examples from the Neighbourhood Area that have been carried out successfully:



Figure 130: - Passive gains and large 16 panel PV array.



Figure 131: - Simply utility upgrade.



Figure 132: - Porch, provides passive heating (Trombe Wall).



Figure 133: - Thermodynamic panels, Ashley Court.



Deliverability

05

5. Deliverability

5.1 Delivery Agents

The Design Code will be a valuable tool for securing context-driven, high quality development in the Pilning & Severn Beach Neighbourhood Area. It will be used in different ways by different actors in the planning and development process, as summarised here:

Applicants, developers and landowners

As a guide to the community and Local Planning Authority expectations on design, allowing a degree of certainty – they will be expected to follow the Guidelines as planning consent is sought.

Where planning applications require a Design and Access Statement, the Statement should explain how the Design Code has been followed.

Local Planning Authority

As a reference point, embedded in policy, against which to assess planning applications.

The Design Code should be discussed with applicants during any pre-application discussions.

Parish Council

As a guide when developing neighbourhood planning policy and commenting on planning applications, ensuring that the Design Code is followed.

Community organisations

As a tool to promote community-backed development and to inform comments on planning applications.

Statutory consultees

As a reference point when commenting on planning applications.



Figure 134: - A simple combination of materials and proportions.

Good design is not an additional cost to development and good placemaking can result in uplifts in value.

The National Planning Policy Framework (paragraph 35) emphasises that a proportionate evidence base should inform plans. Based on a 'positive vision for the future of each area; a framework for addressing housing needs and other economic, social and environmental priorities; and a platform for local people to shape their surroundings' (see paragraph 15). Policies should be 'underpinned by relevant and up-to-date evidence. This should be adequate and proportionate, focused tightly on supporting and justifying the policies concerned, and take into account relevant market signals' (paragraph 31). Crucially planning policies 'should not undermine the deliverability of the plan' (paragraph 34).

Neighbourhood Plans need to be in general conformity with the strategic policies in the corresponding Local Plan. Where new policy requirements are introduced (that carry costs to development) over and above Local Plan and national standards it is necessary to assess whether development will remain deliverable.

The principles and guidance set out in this document and within the Neighbourhood Plan's policies are aligned with national policy and non-statutory best practice on design.

The values and costs of construction will vary based on location, situation, product type, design (architecture, placemaking etc.) and finish; and the state of the market at the point of marketing the properties. The guidelines herein constitute place making principles and guidance to help interpret and apply the statutory policies within the Neighbourhood Plan.

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7. Glossary

Building line: The line formed by the frontages of buildings along a street.

Building line (Formal): buildings aligned with similar distance from the main access.

Building line (Informal): buildings do not align, spaced at different distances from the road.

Built form: Buildings and structures.

Design lineage: To demonstrate a continuation of design character through design that is visibly traceable in appearance to the original building or local vernacular.

Enclosure: The use of buildings and structures to create a sense of defined space.

Enclosure ratio: The enclosure ratio details the spatial character of a street, calculated as the ratio between building façade height and width of street (elevation to elevation distance).

Gateway: The design of a building, site or landscape to symbolise an entrance or arrival to a specific location.

Land Use: What land is used for, based on broad categories of functional land cover, such as urban and industrial use and the different types of agriculture and forestry.

Landscape: An area, as perceived by people, the character of which is the result of the action and interaction of natural and/or human factors.

Landscape Character: A distinct, recognisable and consistent pattern of elements in the landscape.

Listed Building: A listed building is one that has been placed on the Statutory List of Buildings of Special Architectural or Historic Interest. There are three categories of listed buildings in the United Kingdom: Grade I, Grade II* & Grade II.

National Character Area (NCA): A National Character Area is a natural subdivision of England based on a combination of landscape, biodiversity, geodiversity and economic activity.

Nucleated settlements demonstrate a plan arrangement with a central zone or nucleus, which commonly relates to a chronological order of development morphology, but not always.

Offset, Setback or Relief: The space between a building and the road access.

PRoW: Public right of way.

Rural: Relating to, or characteristic of the countryside rather than the town.

Setting: The context or environment in which something sits.

SuDS: Sustainable urban drainage. Used to slowdown the passage of water and often improve water quality.

Tree Preservation Order (TPO): A Tree Preservation Order is an order made by a local planning authority in England to protect specific trees, groups of trees or woodland in the interests of amenity.

Vernacular: The way in which ordinary buildings were built in a particular place, making use of local styles, techniques and materials and responding to local economic and social conditions.

Views: Views that can be seen from an observation point to an object (s) particularly a landscape or building.

